

ISOMETRIC VIEW

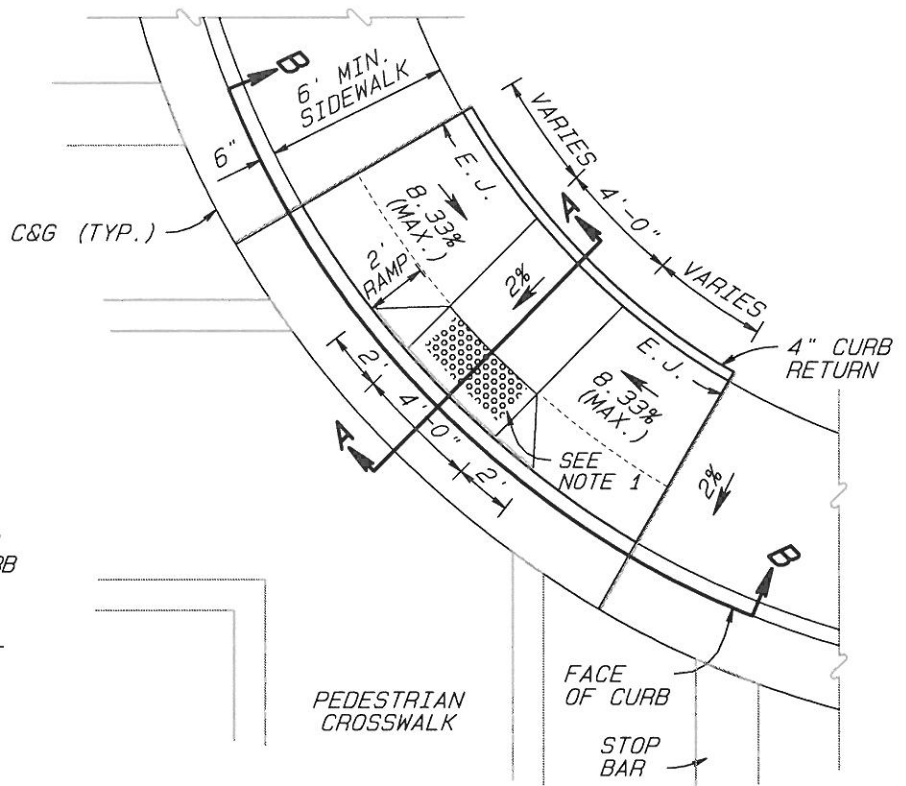
GRASS STRIP WIDTH PLUS S/W WIDTH	GRASS STRIP WIDTH	S/W WIDTH	RAMP WIDTH "W"	CURB RETURN HEIGHT	FLARE WIDTH	MIN. S/W SLOPE LENGTH "L"
6'-0"	0'-0"	6'-0"	2'-0"	4"	2'-0"	4'-0"
6'-6"	1'-6"	5'-0"	2'-6"	4"	2'-6"	4'-0"
7'-0"	2'-0"	5'-0"	3'-0"	4"	3'-0"	4'-0"
7'-6"	2'-6"	5'-0"	3'-6"	4"	3'-6"	3'-6"
8'-0"	3'-0"	5'-0"	4'-0"	4"	4'-0"	3'-0"
8'-6"	3'-6"	5'-0"	4'-6"	N/A	4'-6"	3'-0"
9'-0"	4'-0"	5'-0"	5'-0"	N/A	5'-0"	2'-6"
9'-6"	4'-6"	5'-0"	5'-6"	N/A	5'-6"	2'-6"
10'-0"	5'-0"	5'-0"	6'-0"	N/A	6'-0"	2'-6"

- NOTES:
1. LENGTH OF "W" IS VARIABLE. THE CONTROLLING FACTOR IS THE SLOPE OF "W", WHICH IS 5.0% MIN. AND 8.33% MAX.
 2. MAX. SLOPE ON THE FLARES ARE 10% (10:1)
 3. TRUNCATED DOMES WITH VISUAL CONTRAST TO BE PLACED FROM BACK OF CURB TO 2FT FROM BACK OF CURB
 4. THE ENTIRE CURB RAMPS, FROM EXPANSION JOINT TO EXPANSION JOINT, SHALL BE 6" THICK CONCRETE.

CITY OF GREENSBORO

STANDARD CONCRETE CURB RAMP

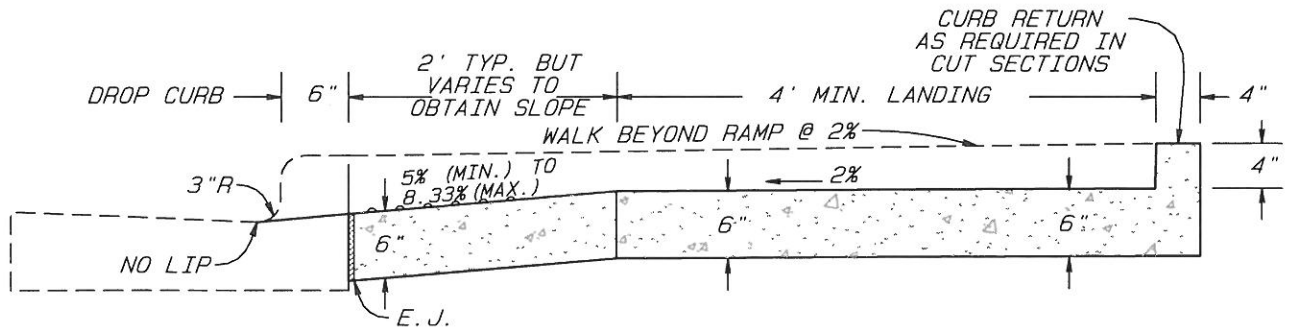
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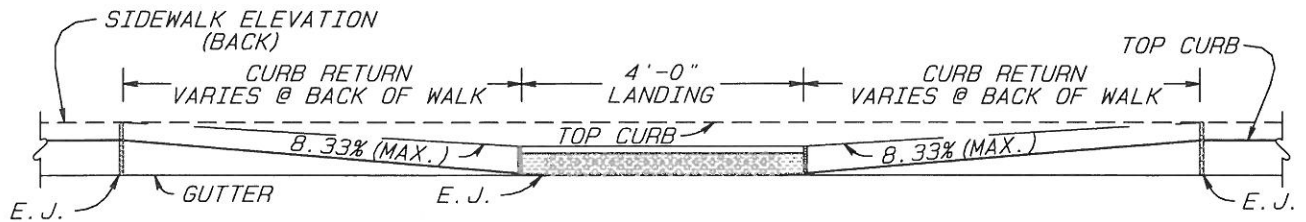
NOTES:

1. TRUNCATED DOMES WITH VISUAL CONTRAST TO BE PLACED FROM BACK OF CURB TO 2 FT FROM BACK OF CURB
2. THE ENTIRE CURB RAMP, FROM EXPANSION JOINT TO EXPANSION JOINT, SHALL BE 6" THICK CONCRETE.

TYPICAL LOCATION DETAIL



SECTION A-A

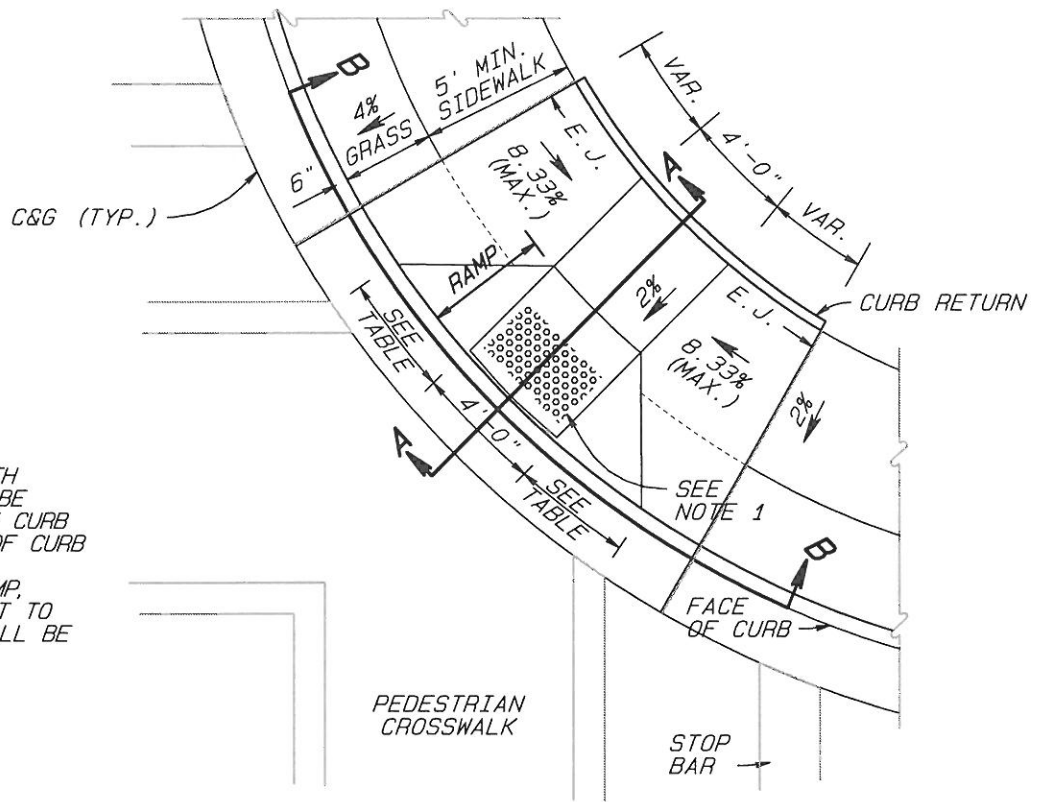


SECTION B-B

CITY OF GREENSBORO

**STANDARD CONCRETE CURB RAMP
(NO GRASS STRIP)**

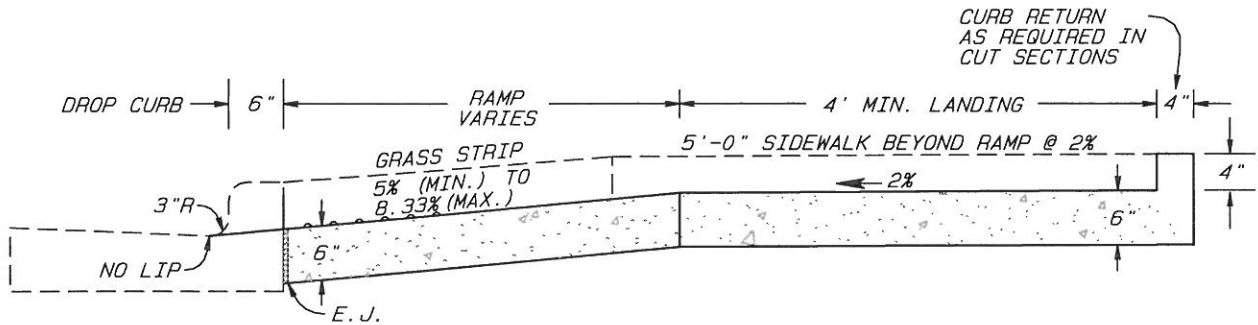
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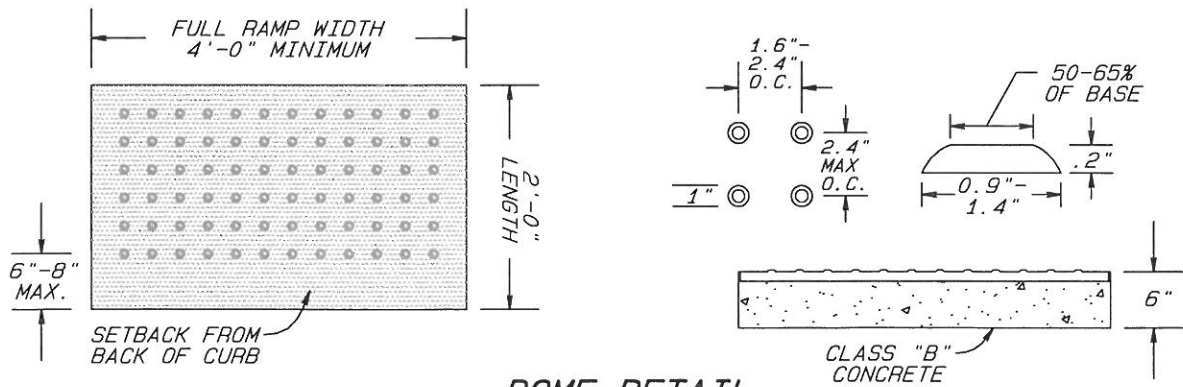
NOTES:

1. TRUNCATED DOMES WITH VISUAL CONTRAST TO BE PLACED FROM BACK OF CURB TO 2 FT FROM BACK OF CURB
2. THE ENTIRE CURB RAMP, FROM EXPANSION JOINT TO EXPANSION JOINT, SHALL BE 6" THICK CONCRETE.

TYPICAL LOCATION DETAIL



SECTION A-A

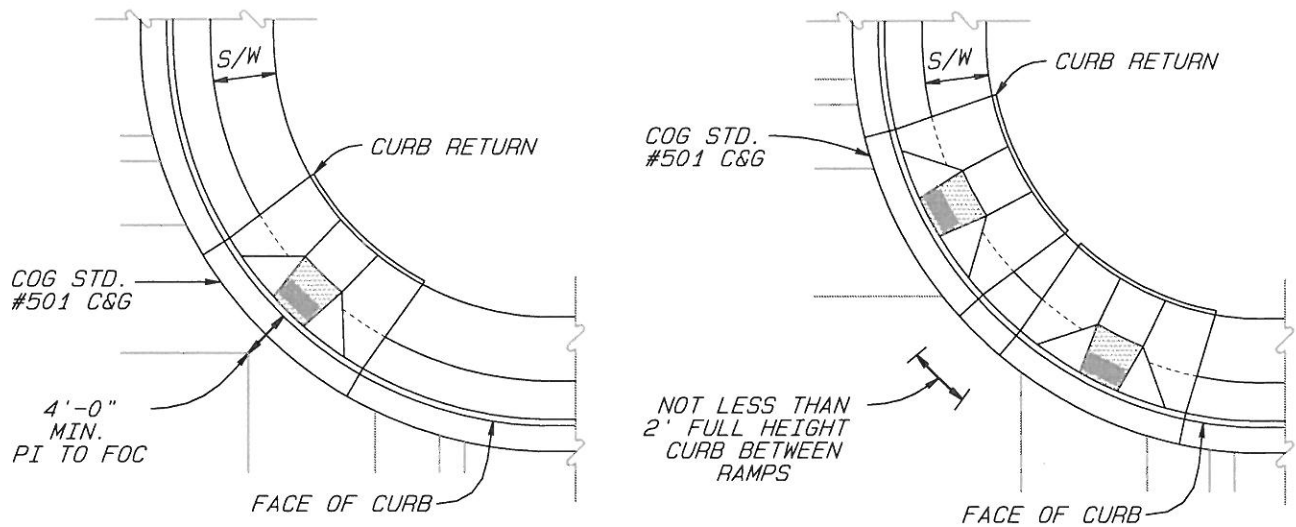


DOME DETAIL

CITY OF GREENSBORO

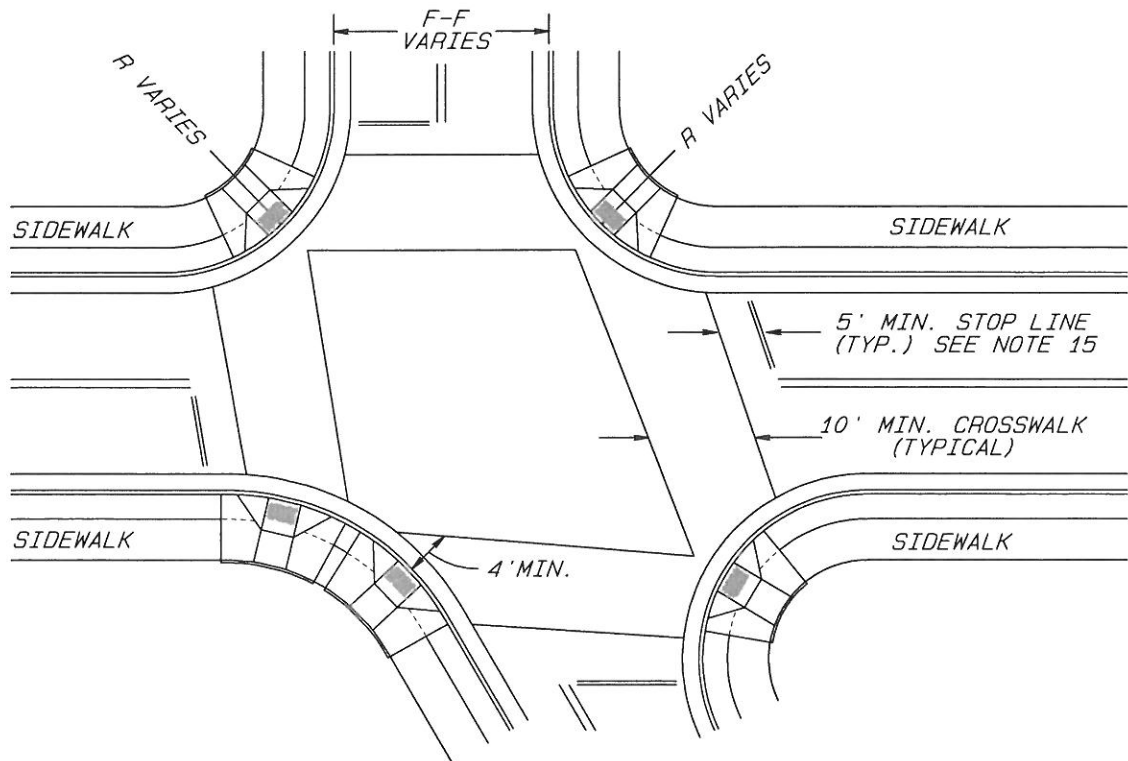
**STANDARD CONCRETE CURB RAMP
(WITH GRASS STRIP)**

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PLAN VIEW

THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SHALL BE COORDINATED SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. DUAL RAMPS WITH FLARED SIDES SHALL BE PLACED SO 24" OF FULL CURB HEIGHT FALLS BETWEEN FLARES.

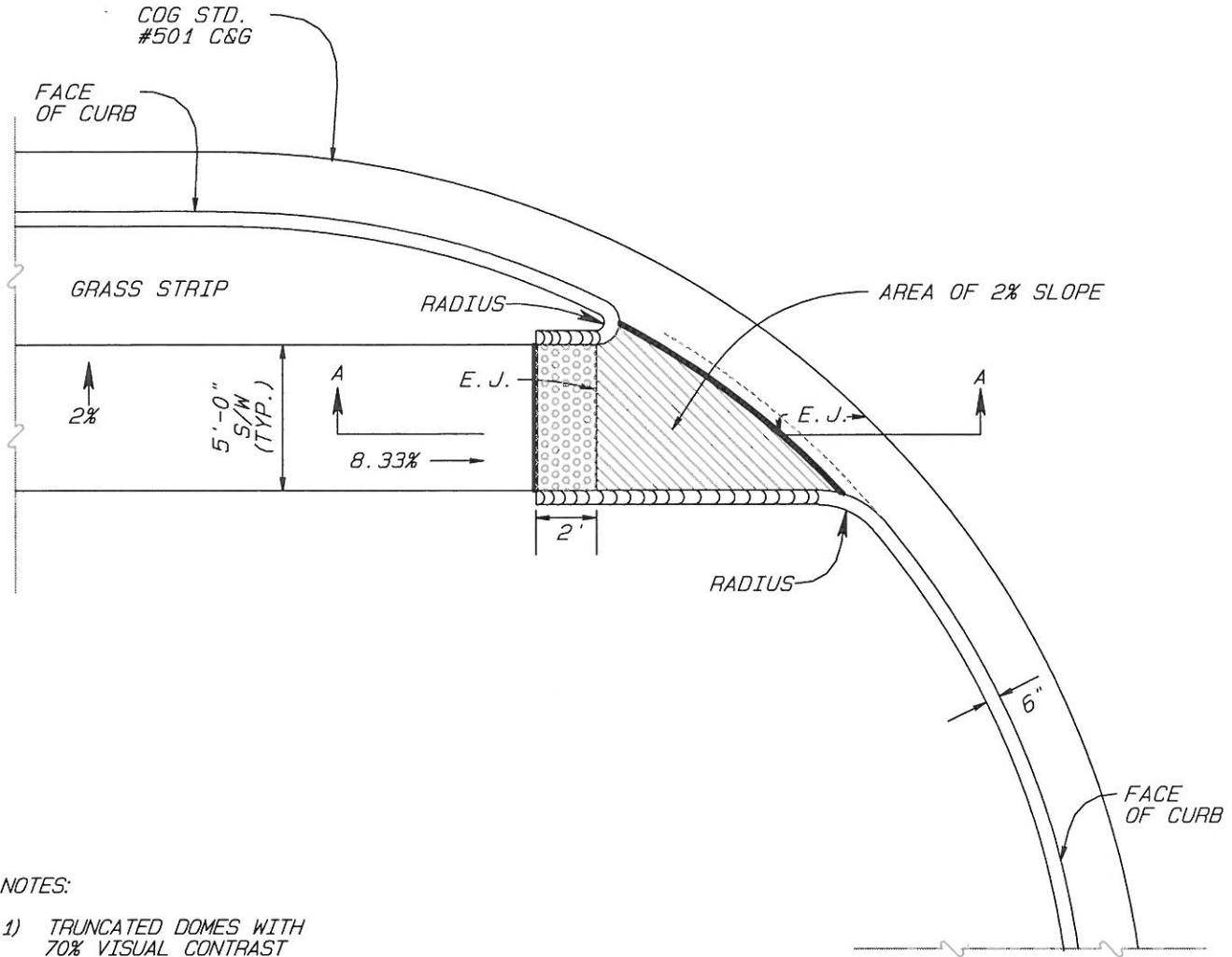


TYPICAL LOCATION DETAIL

CITY OF GREENSBORO

STANDARD CONCRETE CURB RAMP

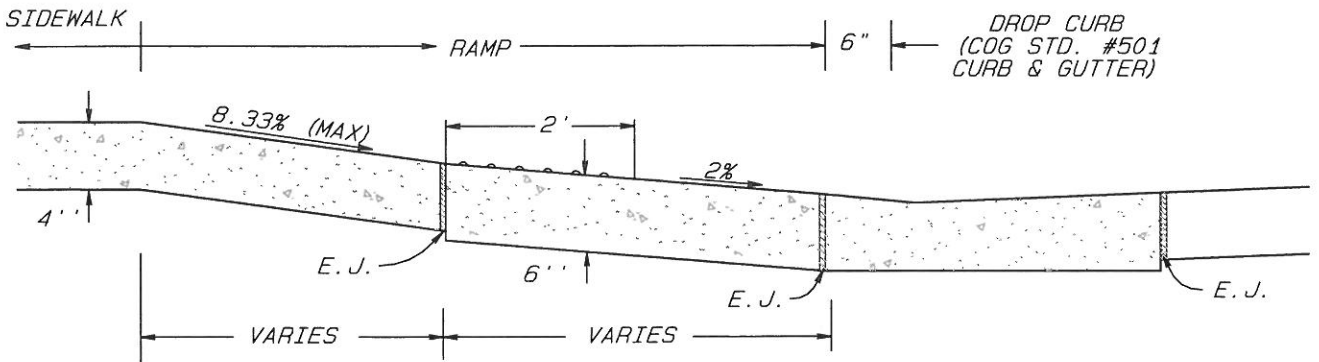
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NOTES:

- 1) TRUNCATED DOMES WITH 70% VISUAL CONTRAST
- 2) SLOPES 12:1 (8.33%) (MAX)

TYPICAL LOCATION DETAIL



E. J. = EXPANSION JOINT

SECTION A-A

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CITY OF GREENSBORO

**STANDARD CONCRETE CURB RAMP
(PARALLEL TO STREET)**

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NOTES:

1. DETECTABLE WARNING SURFACES ARE REQUIRED AT THE BOTTOM OF CURB RAMPS, BORDERS OF MEDIANS AND ISLANDS AND AT RAILROAD CROSSINGS FOR THE FULL WIDTH OF THE RAMP OR SIDEWALK. THE DETECTABLE WARNING SURFACE WILL PROVIDE A 70% CONTRAST TO THE SURROUNDING SURFACE. THE DETECTABLE WARNING SURFACE WILL BE A VITRIFIED POLYMER COMPOSITE (VPC) INSERT WITH TRUNCATED DOMES AS PER THIS STANDARD FOR SIZE AND SPACING REQUIREMENTS. MASONRY INSERTS MAY BE SUBSTITUTED FOR VPC DETECTABLE SURFACES PROVIDED CONSTRUCTION PROVISIONS ARE DETAILED AND APPROVED IN THE CONSTRUCTION PLAN REVIEW. SUBSTITUTE INSERTS SHALL CONFORM TO THE SAME SPACING AND DIMENSIONING REQUIREMENTS.

A MINIMUM 48" X 48" LANDING IS REQUIRED AT THE TOP OF EACH CURB RAMP. THE MAXIMUM CROSS SLOPE ON THIS LANDING IS 2% IN EACH DIRECTION. WARNING SURFACES SHALL MEET OR EXCEED ASTM D 695-072 FOR COMPRESSIVE STRENGTH, ASTM D 638-03 FOR TENSILE STRENGTH, AND ASTM D 790-03 FOR FLEXURAL STRENGTH. WEATHERING OF SURFACE SHALL MEET ASTM D 5420 WITH NO FADING OR CHALKING.
2. CURB RAMPS SHALL BE PROVIDED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. CURB RAMPS SHALL BE LOCATED AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. DO NOT AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED ADJOINING, NOT LESS THAN 2 FEET OF FULL HEIGHT CURB SHALL BE PLACED BETWEEN THE RAMPS.
3. NO SLOPE ON THE CURB RAMP SHALL EXCEED 1"/1' (12: 1) IN RELATIONSHIP TO THE GRADE OF THE STREET.
4. IN NO CASE SHALL THE WIDTH OF THE CURB RAMP BE LESS THAN 48" (4'-0") HOWEVER, WIDTH MAY EXCEED 48".
5. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
6. A 1/2" EXPANSION JOINT WILL BE REQUIRED WHERE THE CONCRETE CURB RAMP JOINS THE CURB AND AS SHOWN ON THIS STANDARD.
7. THE MINIMUM WIDTH OF THE PEDESTRIAN CROSSWALK SHALL BE 10 FEET.
8. STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, SHALL BE USED WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE PARALLEL TO THE INTERSECTING ROADWAY.
9. PARKING SHALL BE ELIMINATED A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
10. ALL PAVEMENT MARKINGS SHALL BE DESIGNED IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.
11. LOCATIONS FOR CURB DROPS OF PROPOSED AND FUTURE CURB RAMPS WILL BE DIMENSIONED ON ALL INTERSECTION IMPROVEMENT PLANS.
12. GAPS DUE TO JOINTS, ETC. SHALL NOT EXCEED 1/2".

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STANDARD CONCRETE CURB RAMP

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