

SOUTH GREENSBORO

AREA PLAN

Existing Conditions,
Demographic &
Background Analysis



GREENSBORO



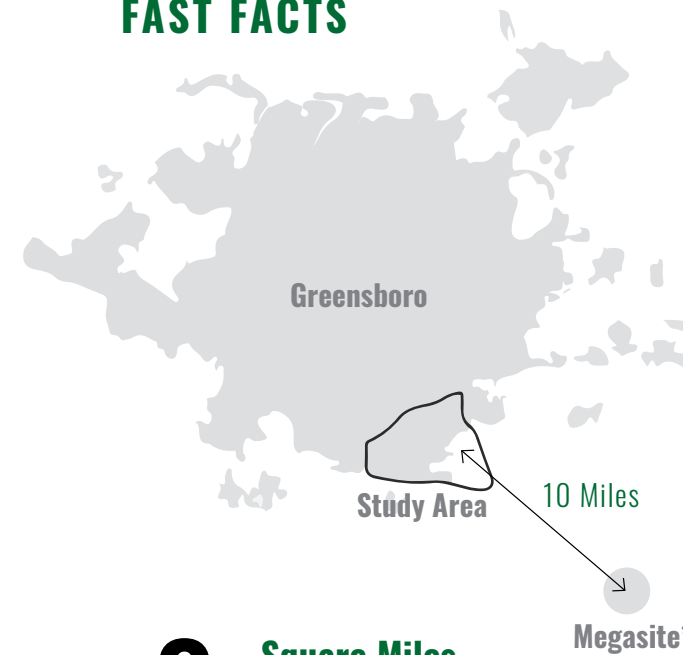
Real People. Real Solutions.

August 2024

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FAST FACTS



- 8** Square Miles in Size
- 19** Neighborhoods
- 7** Parks
- 6** Schools including two public charter schools
- 16** Thousand+ Population
- 10** Miles from Megasite*
*Greensboro-Liberty Megasite, 5,000+ jobs

SUMMARY OF KEY POINTS

- **Crime and appearance is a big concern.**
- **The area is bound and affected by major freeways.**
- **Transit ridership is among the highest in the city, but current frequencies are low.**
- **The northern part of the Study Area is primarily industrial, which negatively impacts quality of life of nearby residential neighborhoods.**
- **The southeastern part of the area is rural, but is poised to experience significant growth in the near future.**
- **The quality of parks and open spaces, flooding, stormwater and environmental issues are a major areas of concern.**
- **Randleman Road is central to everyday life, its the "Main Street" and will continue to play a major role in the area.**
- **Buffalo Creek and its tributaries offer opportunities for access to nature and active mobility, but have no facilities.**
- **Incomes, home values, and educational attainment in the area are significantly lower than the rest of the city.**
- **Engagement is strong and people are dedicated to creating positive change in their community.**
- **There is a general sense in the community that this area has been left behind and doesn't receive equitable attention.**

Study Area

The roughly 8 square mile Study Area sits at the most southern end of the Greensboro city limits and is generally bound by US 220 on the west, US 29/I-40 on the north, US 421 on the east and I-85 on the south.

It is comprised predominately of single family detached residential neighborhoods throughout the central and western portions, a large industrial district along the northern portion and a somewhat rural district outside the city limits sits east of Pleasant Garden Road.

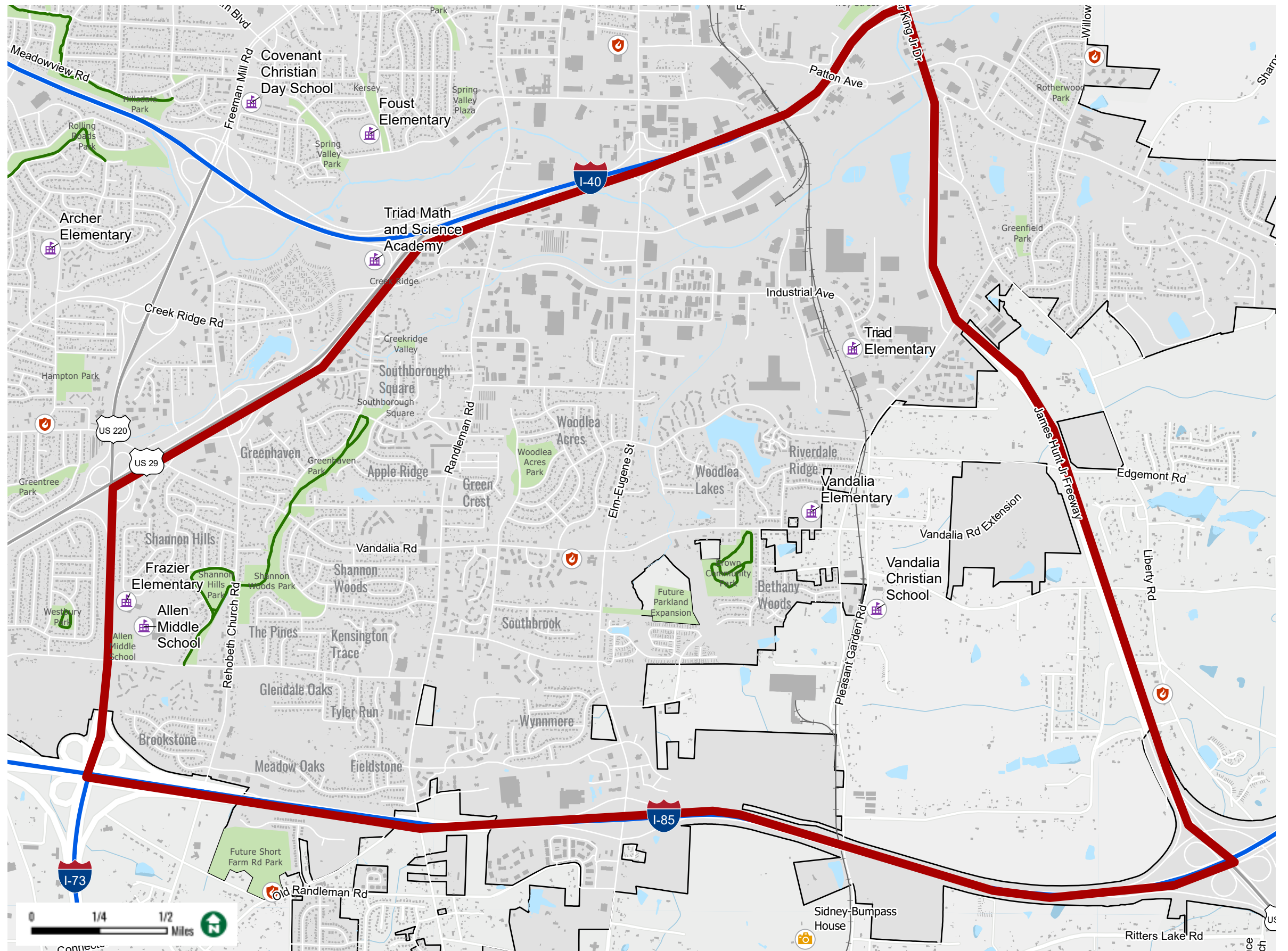
Major roadways between two and five lanes provide connectivity throughout the Study Area:

- Randleman Road
- South Elm-Eugene Street
- Vandalia Road
- Pleasant Garden Road
- Glendale Road
- Creekridge Road

Industrial District

The northern portion of the Study Area hosts some 810 acres (1.27 square miles) of a wide variety of industrial development types with buildings ranging from 8,000 square feet to 430,000 square feet generally. The buildings in this district were developed beginning in the 1950s and the subsequent decades with most having been through the 1980s and a few more recently completed in the 1990s or early 2000s.

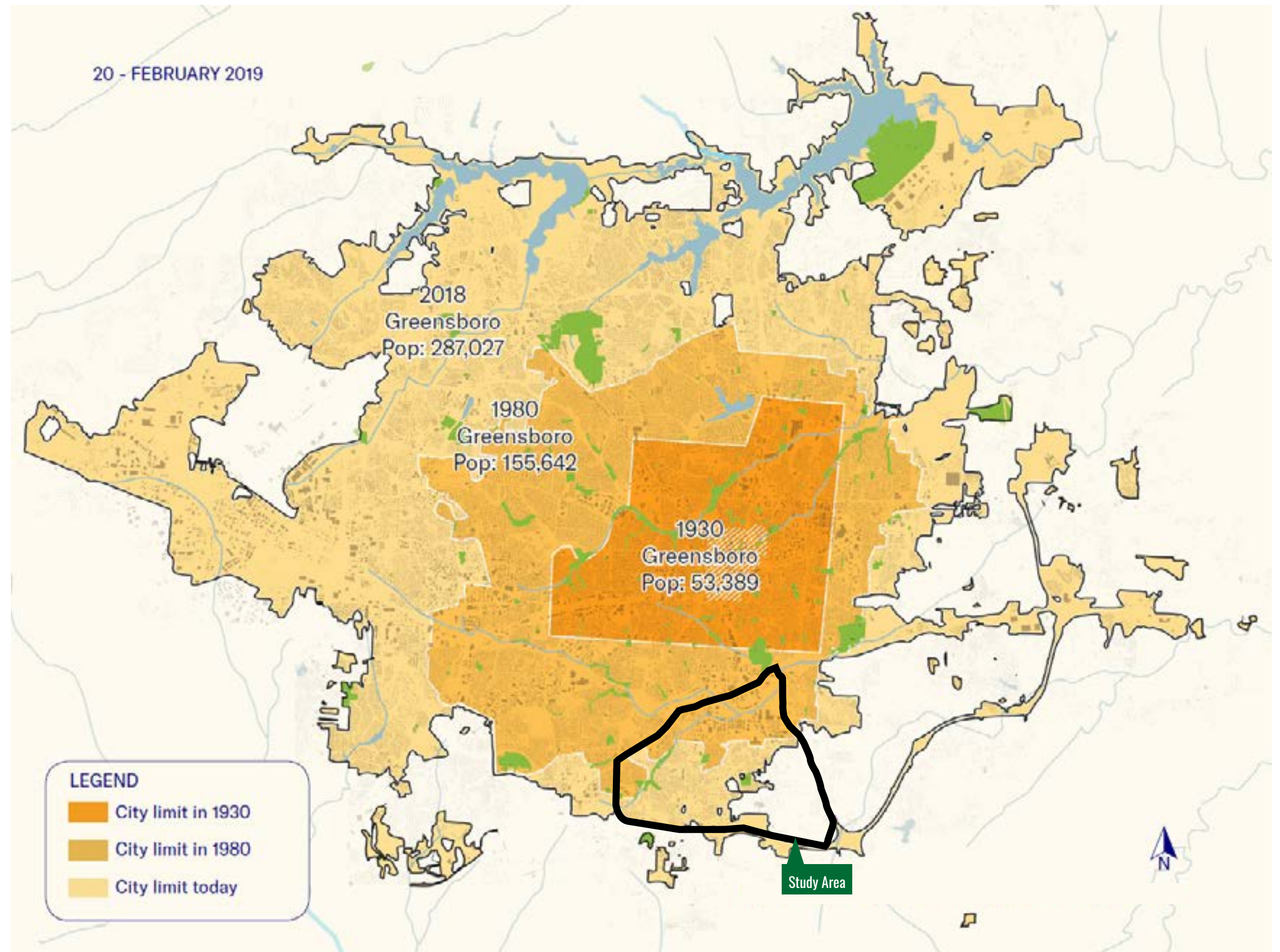
-  Landmarks
-  Schools
-  Fire Stations
-  Places Of Worship
-  City Facilities



History

Growth of Greensboro into the Study Area

The Study Area was primarily located outside the city limits until the 1980s when Greensboro's boundaries began to grow into the Study Area. The area has traditionally been either rural or small residential settlements in the county until industrial development and suburban residential subdivisions began to be constructed throughout the Study Area beginning in the 1960s and continuing through the decades into the early 2000s.



Source: Plan2Play Parks & Recreation Master Plan

Historic Resources

Historic sites recorded by the NC Historic Preservation Office are generally located south of Vandalia Road or east of Pleasant Garden Road. Historic sites are predominately residential homes dating from the mid or late 1800s to the early 1900s, but most structures are no longer present. Some sites have been determined eligible for listing on the National Register, while others have been only studied and have no determination of eligibility.

Two structures of note are still standing and have been determined eligible for listing on the National Register:

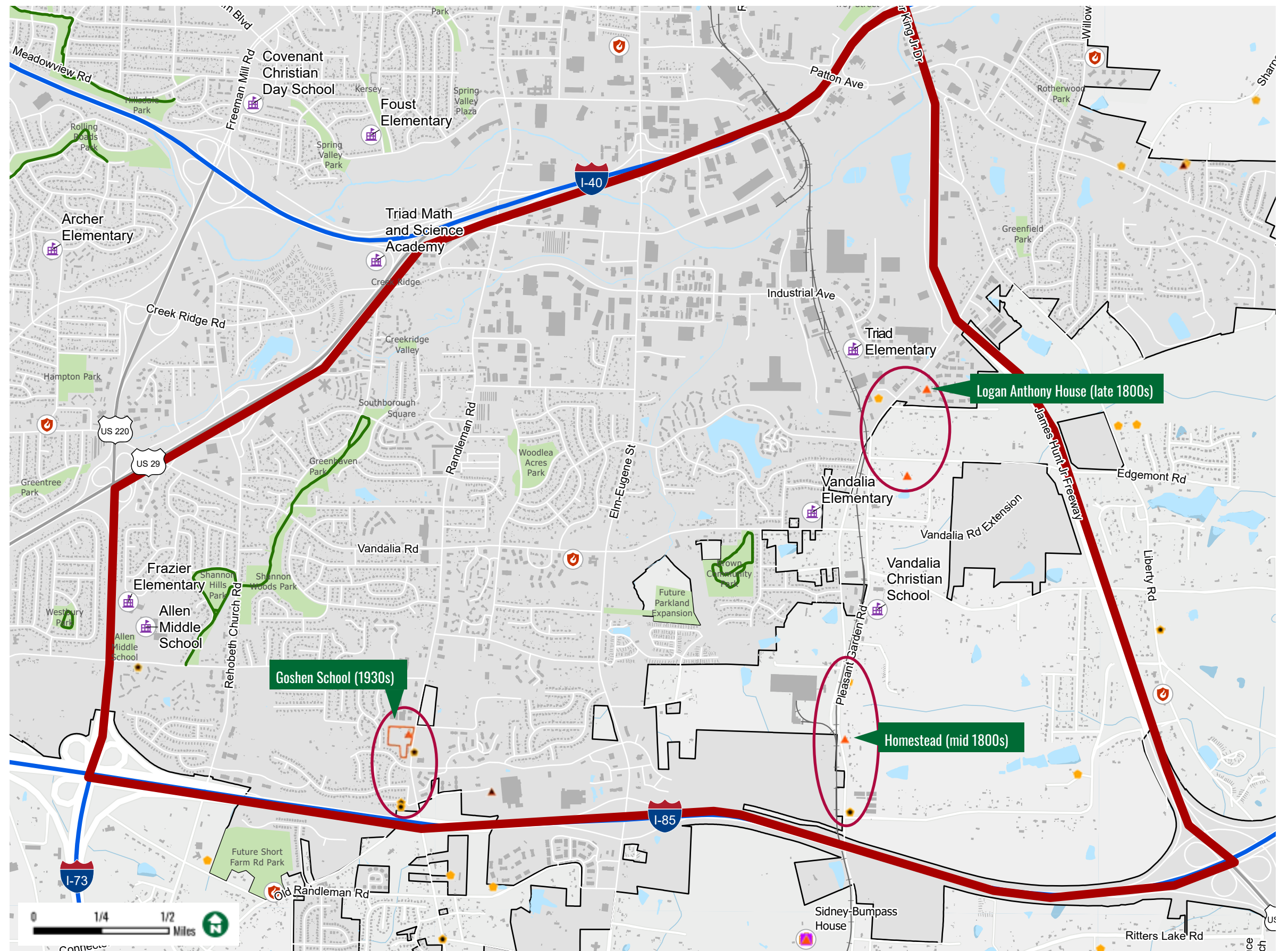
- Logan Anthony House (late 1800s) - incorporated into a contemporary apartment community
- Goshen School (1931) - Rosenwald School; part of the New Goshen United Methodist Church



Logan Anthony House. Source: Google Streetview



Goshen School



- Determined Eligible Boundary
- ▲ Determined Eligible (DOE)
- ▲ DOE, Gone
- Surveyed Only
- Surveyed Only, Gone

Area History

GOSHEN ROSENWALD SCHOOL



New Goshen United Methodist Church Site. Source: Google Earth

Located on Randleman Rd., New Goshen United Methodist Church has a rich and vibrant history that ties the local community into the greater Greensboro community.

Originally constructed in 1888 and burned down in 1896, the church has been a staple for the local community. In 1931, the church donated land to construct a new school, The Goshen Colored School, which was funded by the Rosenwald Rural School Building System, a program that created better access to schools for African Americans in rural and segregated communities

Along with the school, The Goshen Red Wings created a community hub, bringing many residents to the church and school to watch baseball and enjoy cookouts on Saturdays. The church continues to host cookouts and remains and active in the community. In

1947, the team joined the Carolina Negro League and became the official Negro team for the City of Greensboro. Tom Alston, the first basemen for the Red Wings became the first black man to play for the St. Louis Cardinals after being contracted in 1947.

This church not only connected the community but also provided imagery on a national level for the children's book, Tobe, which was written in the late 1930's. This book depicted a new story, following a young African American boy in his rural home. Most of the images taken for use in the book were taken of the community members of the Goshen Church and school.



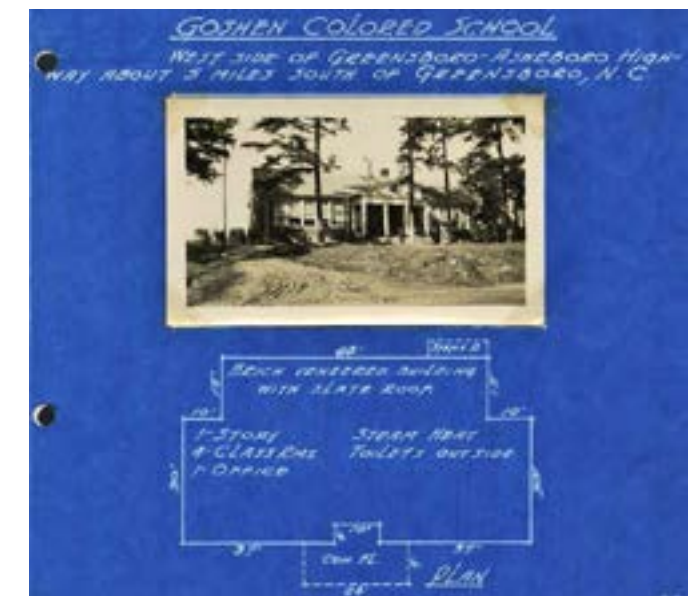
Tom Alston for the St. Louis Cardinals, FindAGrave.com



Image in Tobe, Charles Ferrell, UNC Press



Above: Goshen Rosenwald School on the site of the New Goshen United Methodist Church (Modern day). Right: Historic Plan, Goshen Rosenwald School, Guilford County Fire Insurance Survey



KEY POINTS

- Originally located in the Guilford County Summer Township, a small rural farming community.
- The New Goshen United Methodist Church was established in 1888 and burned down in 1896
- The Church donated land to open an all black school for residents in the 1930's.
- The Goshen Colored School was opened in 1931 and was a Rosenwald School, funded to provide better access to rural African American communities.
- The school became the setting for the book Tobe, one of the first children's book depicting life as a young black farmer in the segregated south.
- The Goshen Red Wings, an all black, semi-professional, baseball team was started at the school and later became the official team for Greensboro in the Carolina Negro League.
- Tom Alston, first basemen of the Red Wings, went on to become the first black man to play for the St. Louis Cardinals in 1947.

People, Housing, and Economy

Demographic Analysis - Who is South Greensboro?

16,045 People

6,935 Households

665 businesses, employing a total of 7,211 people

Median Home Value \$171,381 (\$260,744 Citywide)



Images collected from Google Maps of actual people, places, and events in the Study Area.

Tapestry Segmentation for South Greensboro

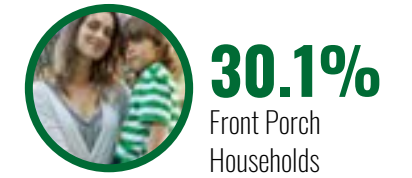
Esri Tapestry Segmentation is a market segmentation system that uses an array of variables to describe the characteristics of diverse communities. Each community classification is called a segment, and segments are aggregated into LifeMode and Urbanization groups. LifeMode groups further classify segments based on shared characteristics relating to lifestyle, life stage, housing, and households, while Urbanization groups further classify segments based on location-based commonalities (ESRI).



Metro Fusion households are identified as young and mobile residents who do not speak fluent English but are dedicated to their work and families.

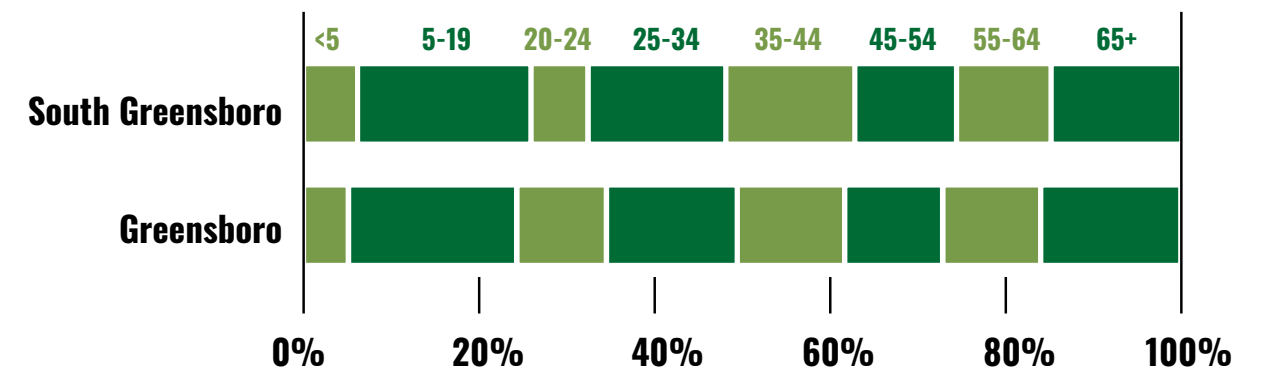


Family Foundations households are identified in established neighborhoods as older families, with children still living at home who are looking to become more financially stable.

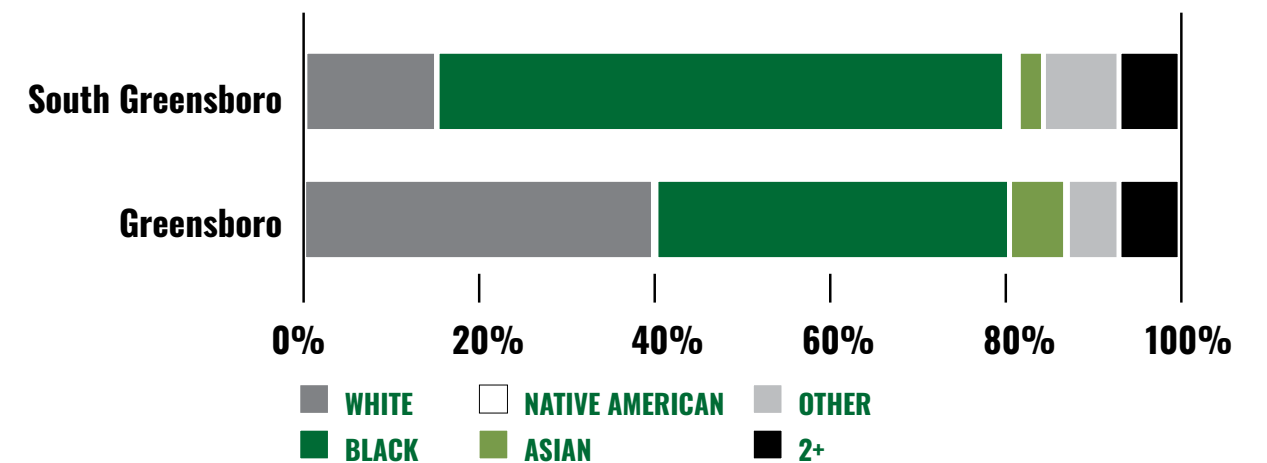


Front porch households are identified as young families and single-resident households that are most commonly renters

Population by Age (2023)



Population by Race (2023)



SOUTH GREENSBORO INFORMATION

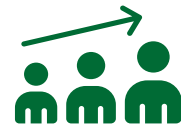
Population, Household Size, and Average Age



16,045
Population



2.3 Persons
Average Household Size



36.3 Years Old
Average Age

Top Lifestyle Spending Categories



\$1,801
Travel



\$37
Entertainment



\$91
Audio Products



\$44
Performing Arts



\$44
Performing Arts



\$56
Sports Events

Commute



16.1%

of residents in South Greensboro spend **7+ hours a week** commuting to and from work.

Job Types

"Blue Collar" describes jobs like construction or mechanics, while "white collar" describes jobs in office settings. There is a larger population of blue collar workers in South Greensboro when compared to the entire city of Greensboro.

South Greensboro Job Types



15.2%
Services



27.8%
Blue Collar



57%
White Collar

City of Greensboro Job Types



16.3%
Services



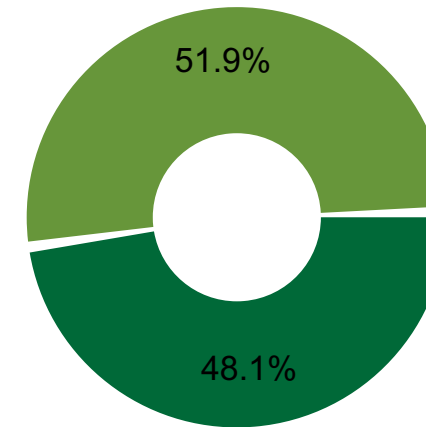
21.2%
Blue Collar



62.6%
White Collar

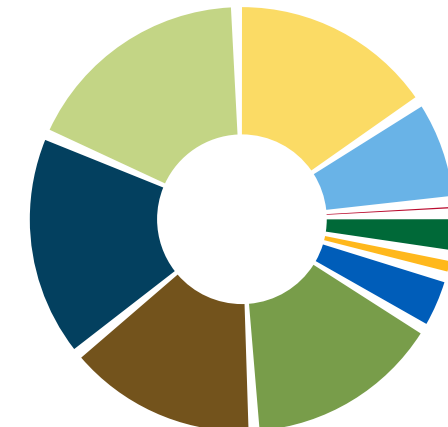
South Greensboro

Home Ownership



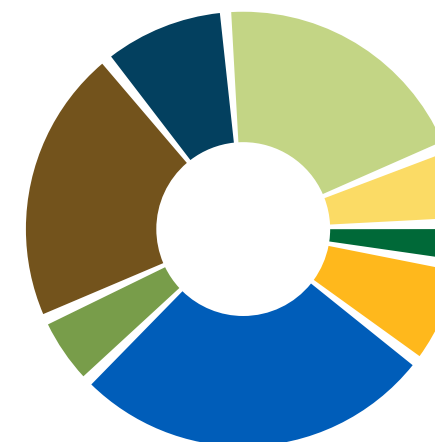
● Own ● Rent

Housing Year Built



● <1939 ● 1940-49 ● 1950-59
● 1960-69 ● 1970-79 ● 1980-89
● 1990-99 ● 2000-09 ● 2010-19
● ≥ 2020

Educational Attainment



● < 9th Grade ● No Diploma
● HS Diploma ● GED
● Some College ● Assoc Degree
● Bach Degree ● Grad Degree













Mortgage Burden

- The data indicates higher mortgage burden in the Study Area vs the city overall.
- Most residents in both the Study Area and the city of Greensboro spend 10-14% of their salaries on mortgage payments.
- While the Study Area has a higher number of residents allocating 20-24% of their salaries to mortgages, the majority still pay below the national average 29% threshold.

© 2024 Esri

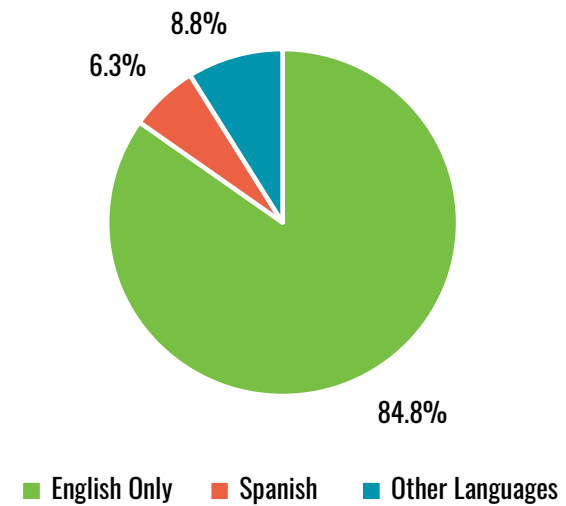
Source: U.S. Census Bureau, 2018-2022 American Community Survey

DATA COMPARISON: SOUTH GREENSBORO TO THE CITY

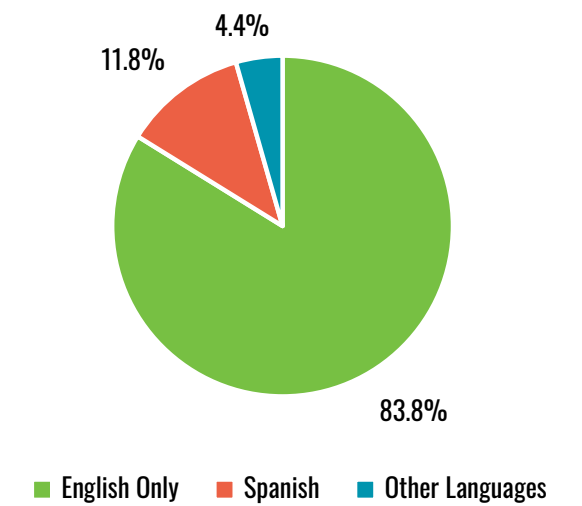
	Greensboro	South Greensboro	
 Median Household Income	\$59,630	\$55,397	
 Median Net Worth	\$82,841	\$56,563	Significantly lower than city; could indicate fewer valuable assets, lower generational wealth
 Per Capita Income	\$33,383	\$29,805	
 Median Home Value	\$260,744	\$171,381	Significantly lower than city; represents affordability, but also impacts net worth
 Married Residents	41%	32.1%	
 Residents Age 65+	16.2%	14.5%	
 Households with a Disabled Resident	21.4%	22.4%	
 Households Receiving Food Assistance	20%	21.0%	Significantly higher than national and state average of around 12%
 Households With No Internet Access	2.8%	14.6%	Double the national average
 Households without an Automobile	7.24%	6.69%	
 Unemployment Rates	4%	5.3%	May indicate higher levels of underemployment, or a larger proportion of the population not participating in the labor force
 Employee to Resident Ratio	64:100	42:100	

Source: U.S. Census Bureau, 2018-2022 American Community Survey

City of Greensboro Languages Spoken



South Greensboro Languages Spoken



RETAIL GOODS AND SERVICES

Spending Potential Index (SPI)

Data Note: The Spending Potential Index (SPI) is household-based, and represents the amount spent for a product or service relative to a national average of 100. Values above 100 may indicate opportunity for more retail supply in those categories. Lower values may indicate a lack of demand for those retail categories, as the ability of consumers to buy those is low.

- Spending potential index in all categories is well below national average of 100
- Highest spending potential index categories:
 - Dating services – 80
 - Smoking products - 80
 - Satellite dishes – 79
 - Video games – 77
 - Prescription drugs - 74
 - Children's apparel - 72
 - Admission to sporting events – 70

Source: U.S. Census Bureau, 2018-2022 American Community Survey

Existing Retail Supply

In order to determine a "best guess" at current retail supply in the Study Area, we totaled building footprint square footages that were located in "Commercial" land use data provided by the City of Greensboro.

We found about **1.95 million square feet** of retail square footage in the Study Area. The population of the Study Area is about **16,000** people.

The average retail square footage per capita in the US is **23 sf/person**. Using that average, the 1.95 million square feet of retail in the Study Area could supply a population of about **85,000** people.

By that rationale, the Study Area is either oversupplied with retail, or the retail in the Study Area should be supporting a population area that is more than **5x larger** than the Study Area's population.

Community Assets

South Greensboro's Community Assets include public facilities like greenways, parks and fire stations as well as daily needs like neighborhood retail centers and places of worship.

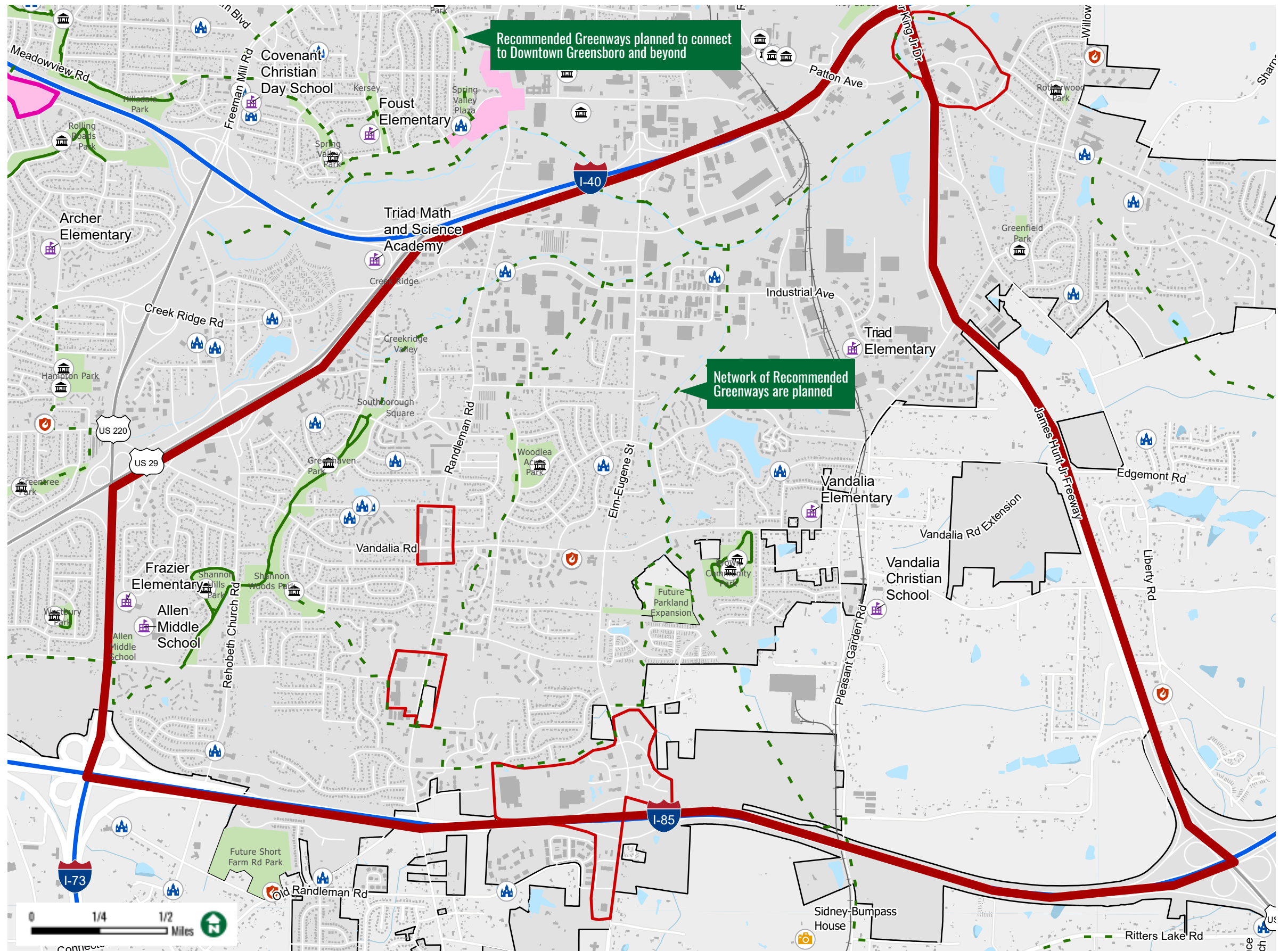
There are several neighborhood parks throughout the Study Area and an existing 1.25 miles of greenway near Shannon Hills and Greenhaven parks. Of note are the Recommended Greenways not yet built. They form an active mobility network of off street paths that would connect most parts of the Study Area to each other as well as to the broader trail network that extends northward toward downtown and other destinations in the city. They also provide key connections to Activity Centers within and beyond the Study Area, which are places where daily needs like pharmacies, retail and restaurants can be found.



Existing greenway in Greenhaven Park

Activity Centers

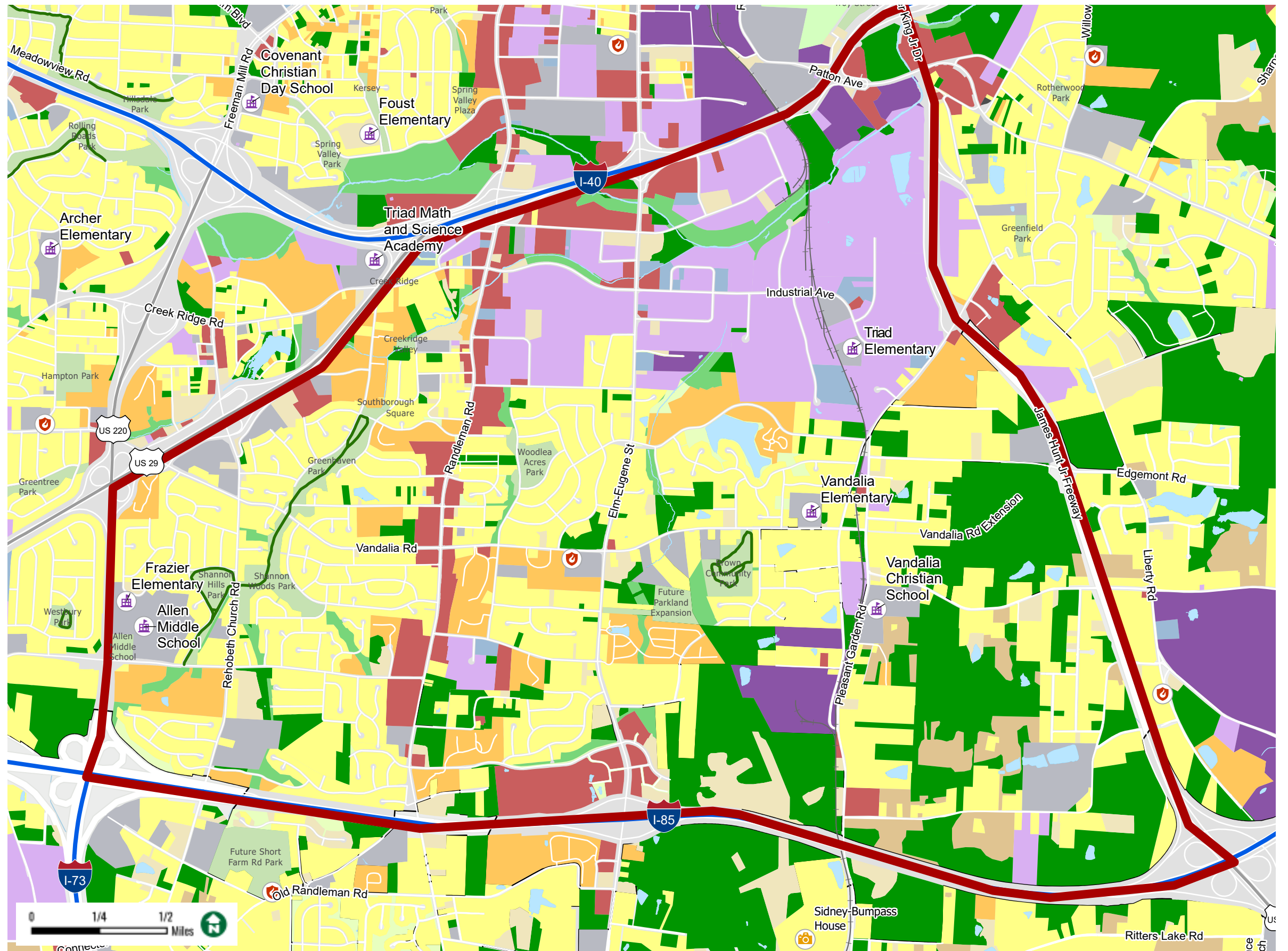
- Neighborhood Scaled Center
- District Scaled Center
- Regional Scaled Center
- Landmarks
- Schools
- Fire Stations
- Places Of Worship
- City Facilities



Land Use

The predominate land use across the Study Area is Single Family residential at almost 30% followed by Woods at 15% and Light Industrial at 12%. Multifamily uses are spread across the Study Area with some clustered around the Randleman Road corridor.

Land Use	Land Use Acres	Percent of Study Area (Acres)
Agriculture	99.52	1.96
Commercial	265.81	5.25
Heavy Industrial	94.77	1.87
Institutional	221.04	4.36
Light Industrial	634.31	12.52
Multi Family	335.31	6.62
Office	43.96	0.87
Open Space	153.27	3.03
Owner Association Open Space	28.03	0.55
Park	203.89	4.03
Single Family	1450.07	28.63
Water	62.56	1.24
Woods	763.44	15.07
Rights-of-way	709	14.00
Study Area Total Acres	5065	100.00



Crime

Past planning documents and community engagement results noted a distinct impression that crime was a major issue in the Study Area. The research also reported that regardless of the actual number of crimes and the occurrence and prevalence of crime in the Study Area, it is consistently viewed as a deterrent for improvement and investment in the Study Area.

Clusters of crime are highest at big box retail locations throughout the Study Area. For example, some 600+ crimes were located at the Walmart near Elmsley Drive in the southern part of the Study Area.

Crime appears to be spread across the Study Area with some increase in clusters and violent crimes closer to Randleman Road. No crimes are shown east of Pleasant Garden Road outside the city limits, as data from the Guilford County Sherriff Department has not yet been retrieved.

Greensboro Police Department Crime Data 2020-2023

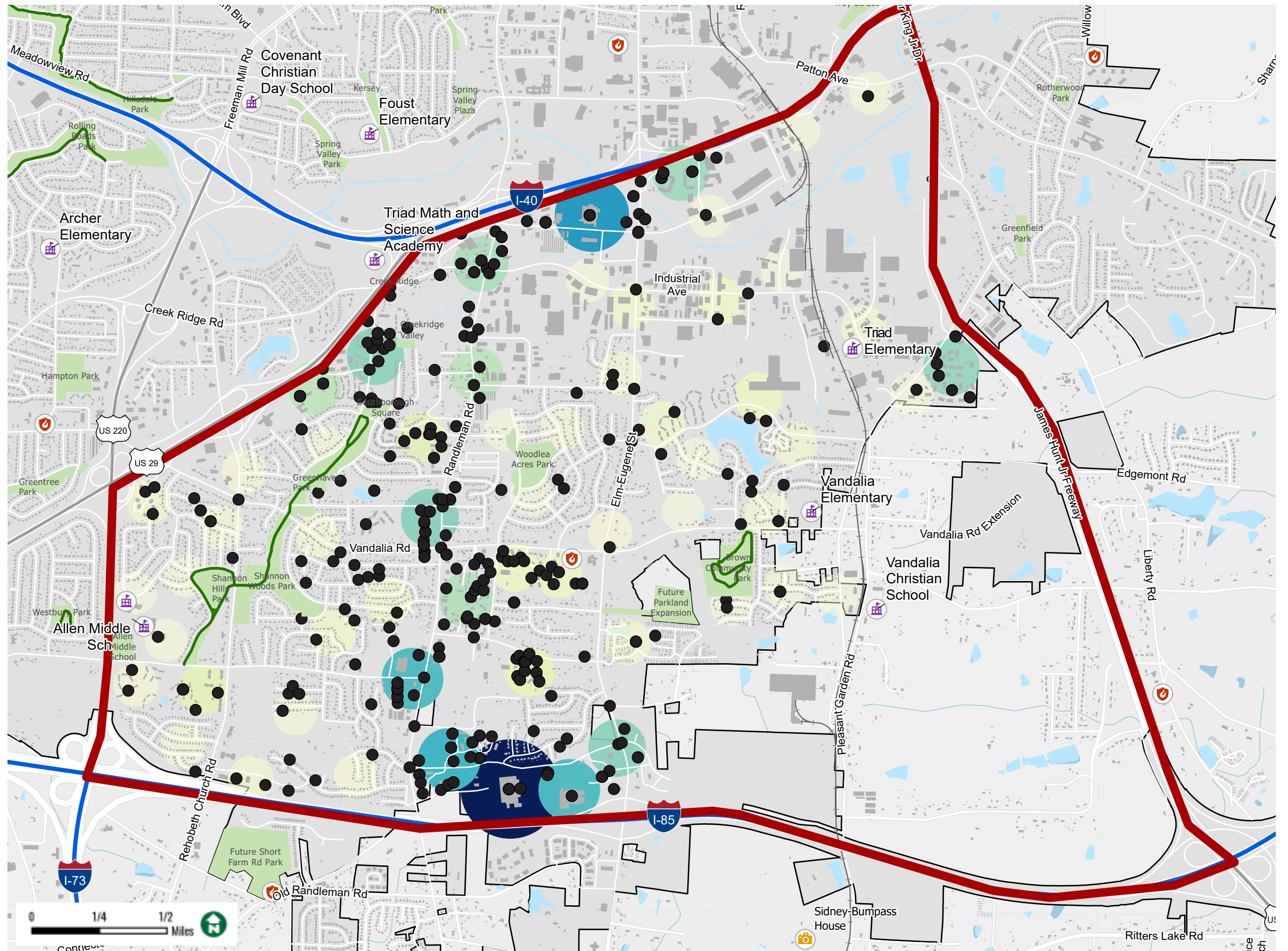
All General Crime

Clusters

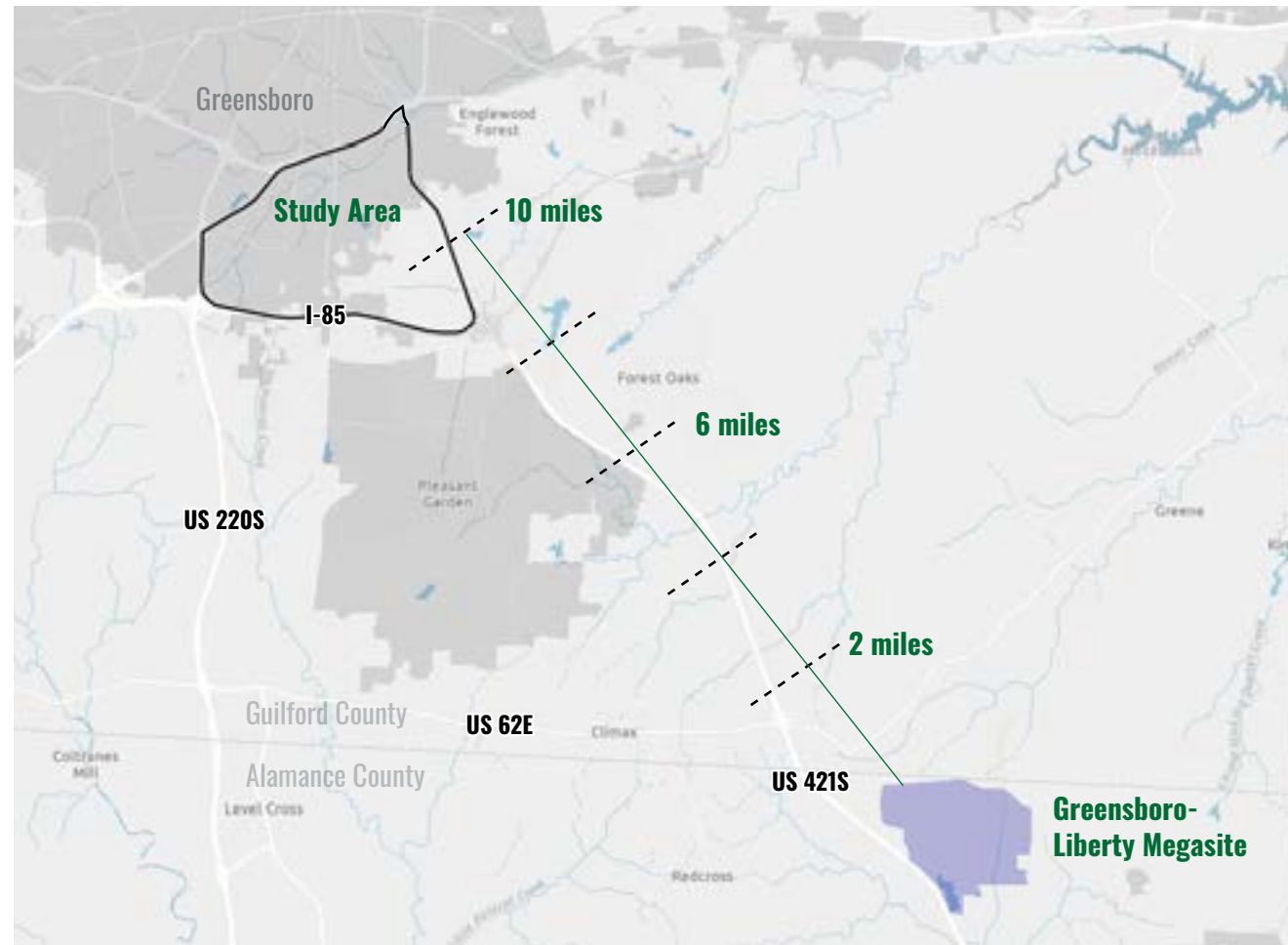


● Violent Crime

Aggravated assault, criminal homicide, rape, and robbery



Greensboro-Liberty Megasite

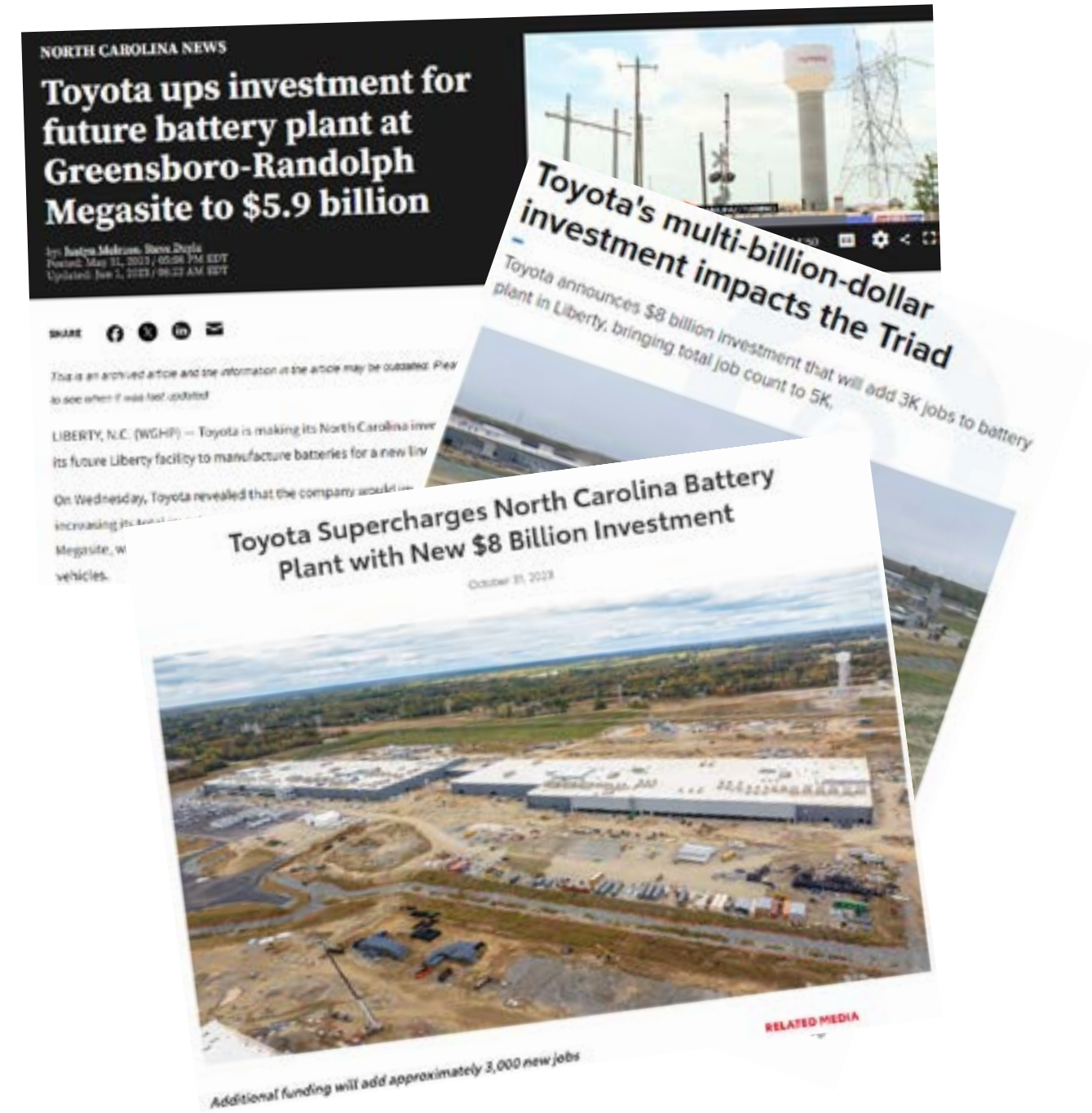


- **Megasite selected for automotive parts production**
- **Significant economic development influence in the area**
- **\$8+ billion investment; 5,000+ jobs**
- **3,000+ additional jobs projected after construction of the megasite is completed (currently underway)**
- **Site is driving utility and infrastructure improvements along 62E and 421S corridors**
- **Site and imminent jobs are driving up development interests in surrounding areas for miles due to speculation of demand for housing for new employees, new related manufacturing, and commercial**
- **Proximity to Study Area suggests megasite has strong potential to affect residential, industrial, and commercial development interests in the Study Area and along the 421S corridor**

A megasite is a large parcel of developable land that is considered “shovel-ready” and can quickly break ground on large industrial operations.

North Carolina’s multiple strategically located megasites offer advanced manufacturers the perfect opportunity to grow with the lowest corporate income tax in the US, an abundance of skilled workers, and convenient access to major markets, consumers, and suppliers.

Source: Economic Development Partnership of North Carolina (EDPNC)



Planning & Zoning

Zoning

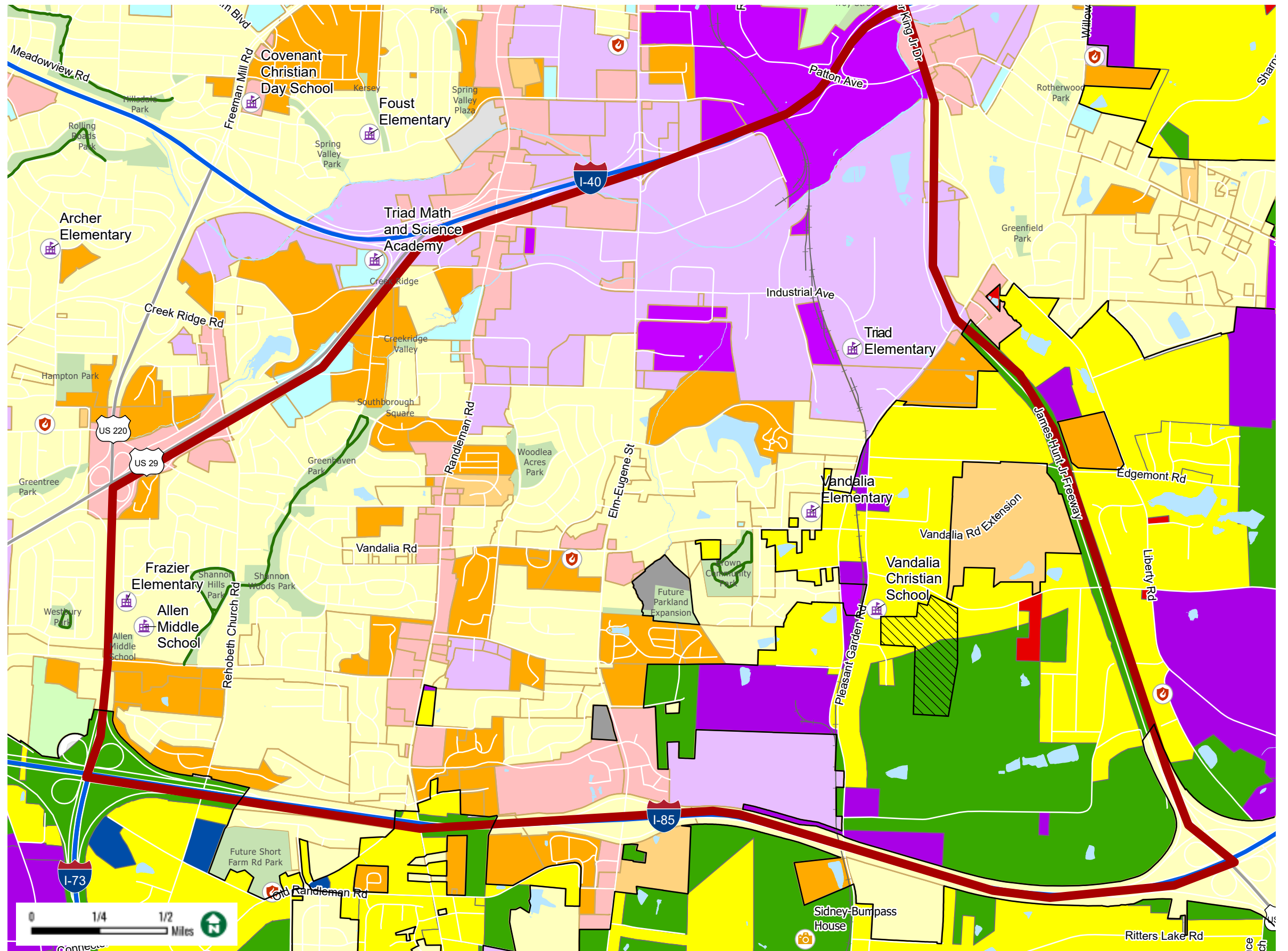
Zoning in the Study Area is relatively consistent with Land Use, as previously described herein. Single family residential zoning is most dominant throughout the Study Area with light industrial following. A cluster of large tracts in the unincorporated portion of the Study Area near I-85 and 421S are zoned Agricultural and one property east of Vandalia Christian School is classified as a Voluntary Agricultural District.

Greensboro

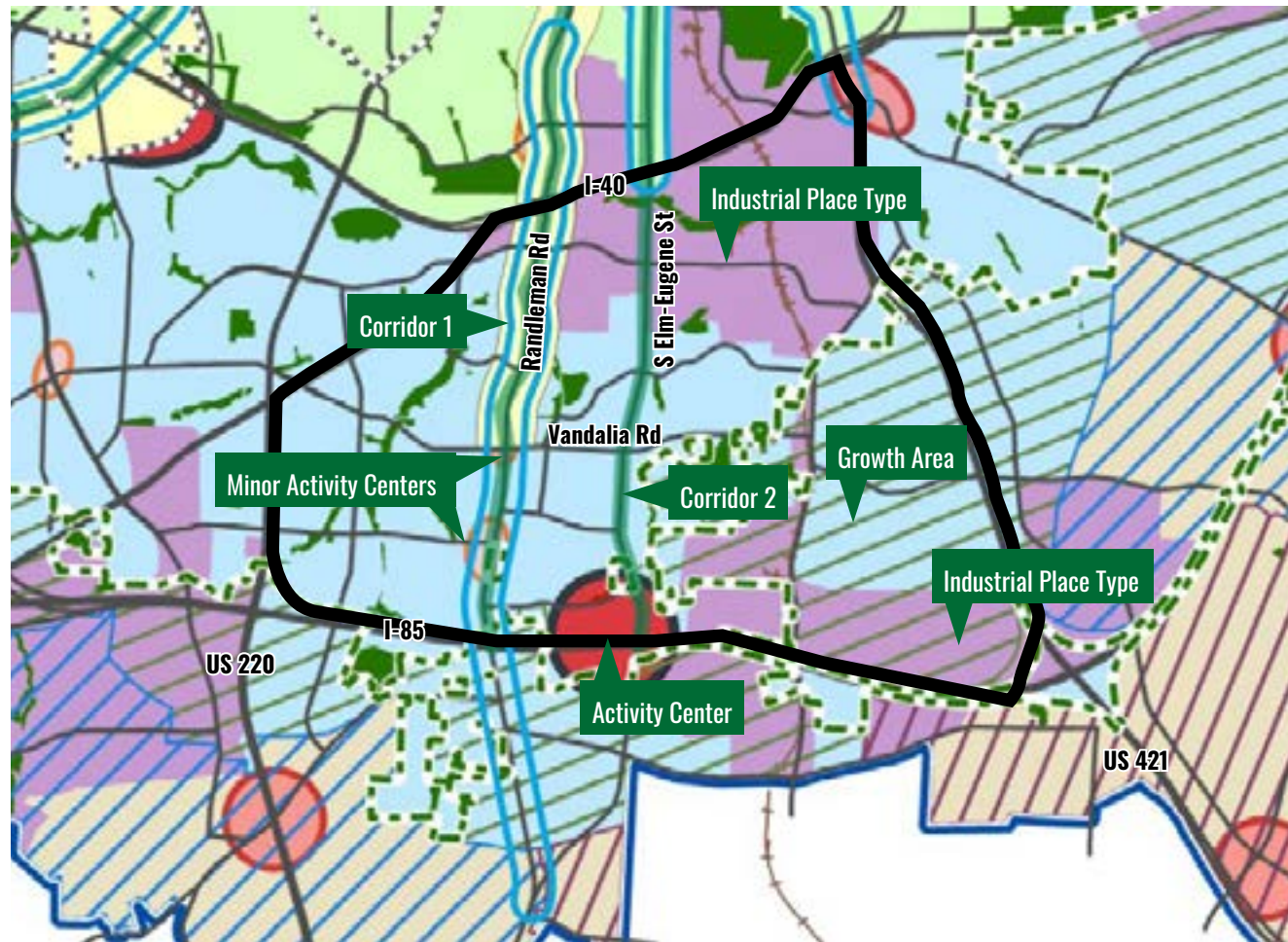
- Parkland and Natural Resource Area
- Public and Institutional
- Commercial
- Office
- Light Industrial
- Heavy Industrial
- Auto-Oriented
- Single-family Residential
- Multi-family Residential
- Planned Unit Development

Guilford County

- Public Institution
- Neighborhood Business
- Light Industrial
- Agricultural
- Residential Single Family - 1 unit/acre
- Voluntary Agricultural District



Future Built Form - Greensboro

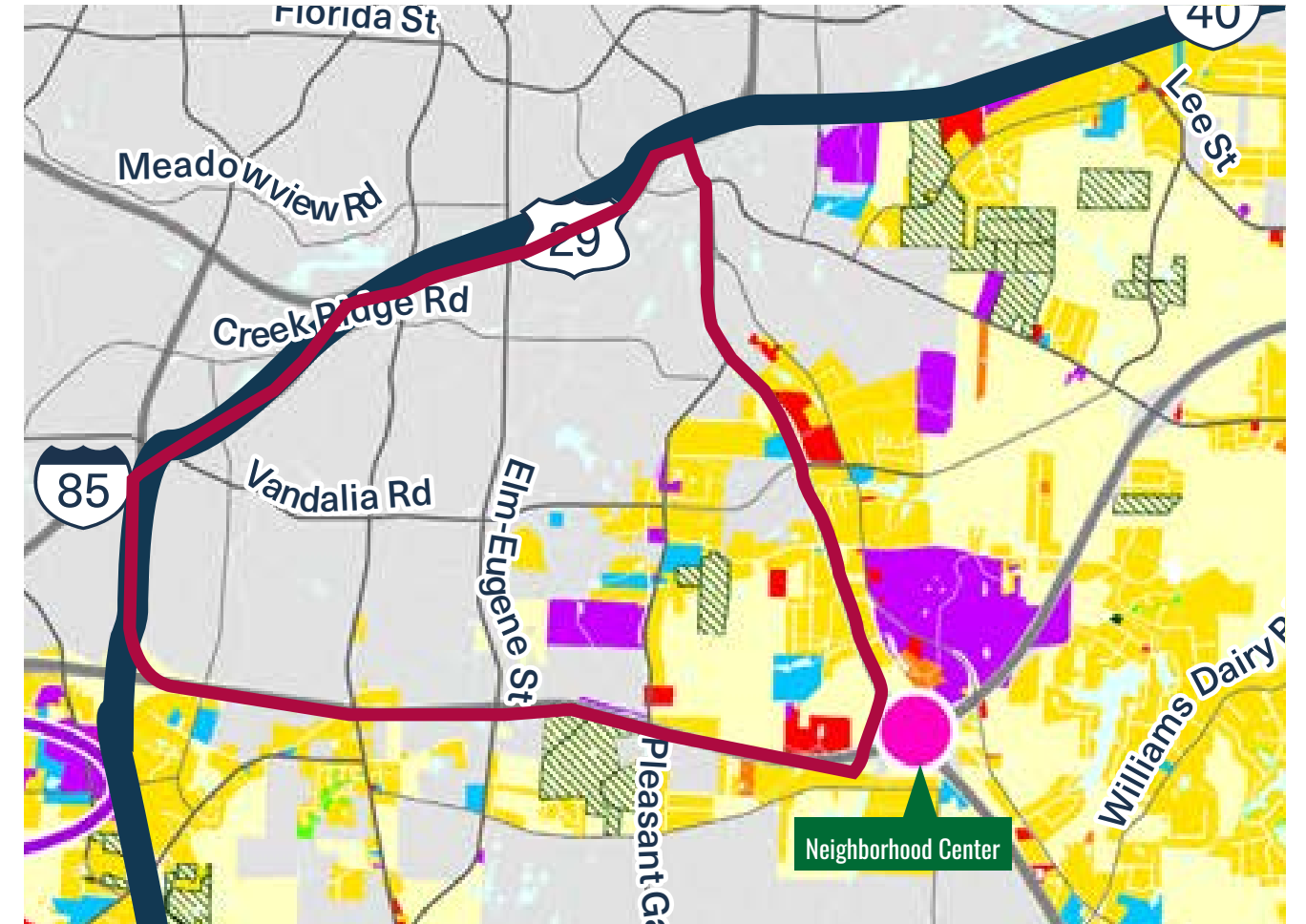


The GSO2040 Comprehensive Plan mapped out areas to show how they were envisioned to grow and develop in ways to help create a more desirable and sustainable Greensboro.

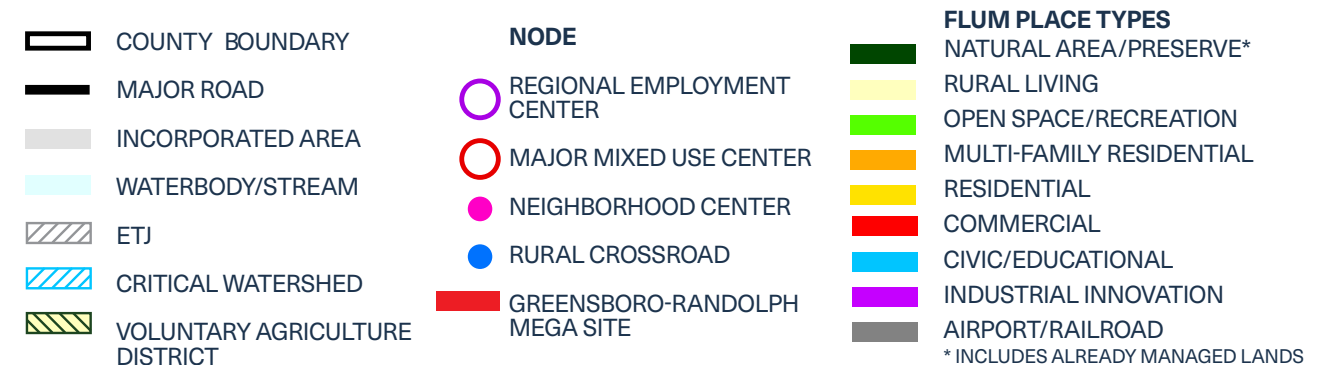
The Study Area includes Reinvestment and Urban Mixed Use Corridor designations on Randleman Road, existing Activity Centers are envisioned to continue and the southeastern portion of the Study Area in Guilford County is labeled as Growth Tier 1. Growth Tier 1 indicates the ability for the City to provide utilities and public services. That area is also designated as a Planned Industrial District, which is inconsistent with Guilford County's Draft Future Land Use Vision on the following pages.



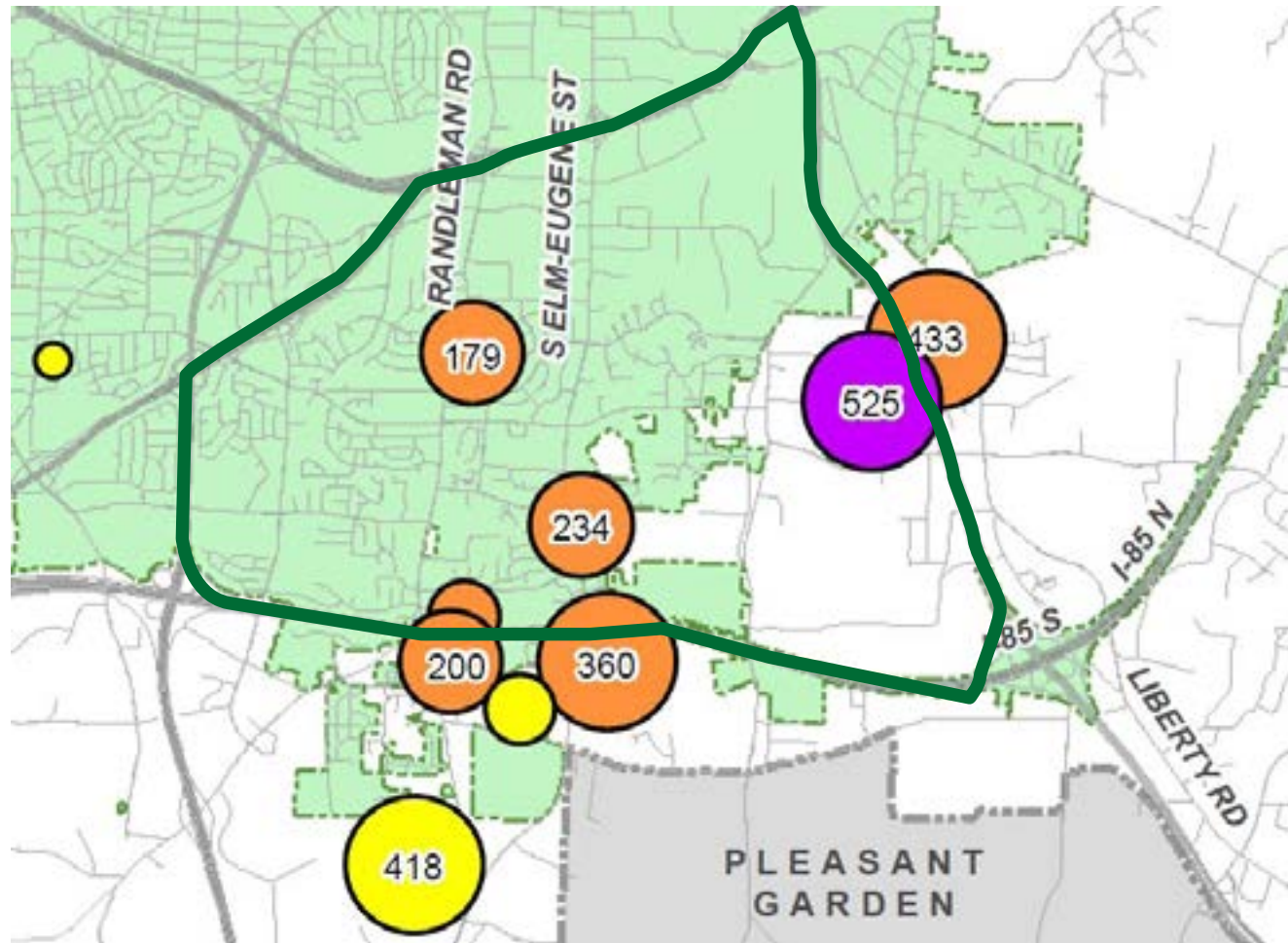
Future Land Use (Draft) - Guilford County



Guilford County's Draft Comprehensive Plan includes a Draft Future Land Use Map (FLUM shown above). The southeastern portion of the Study Area within Guilford County is envisioned to include a mix of commercial, civic/educational, and residential uses, which are inconsistent with Greensboro's Future Built Form envisioned on the opposite page. Also of note is a planned Neighborhood Center at the intersection of US 421S and I-85.



Approved Housing Units

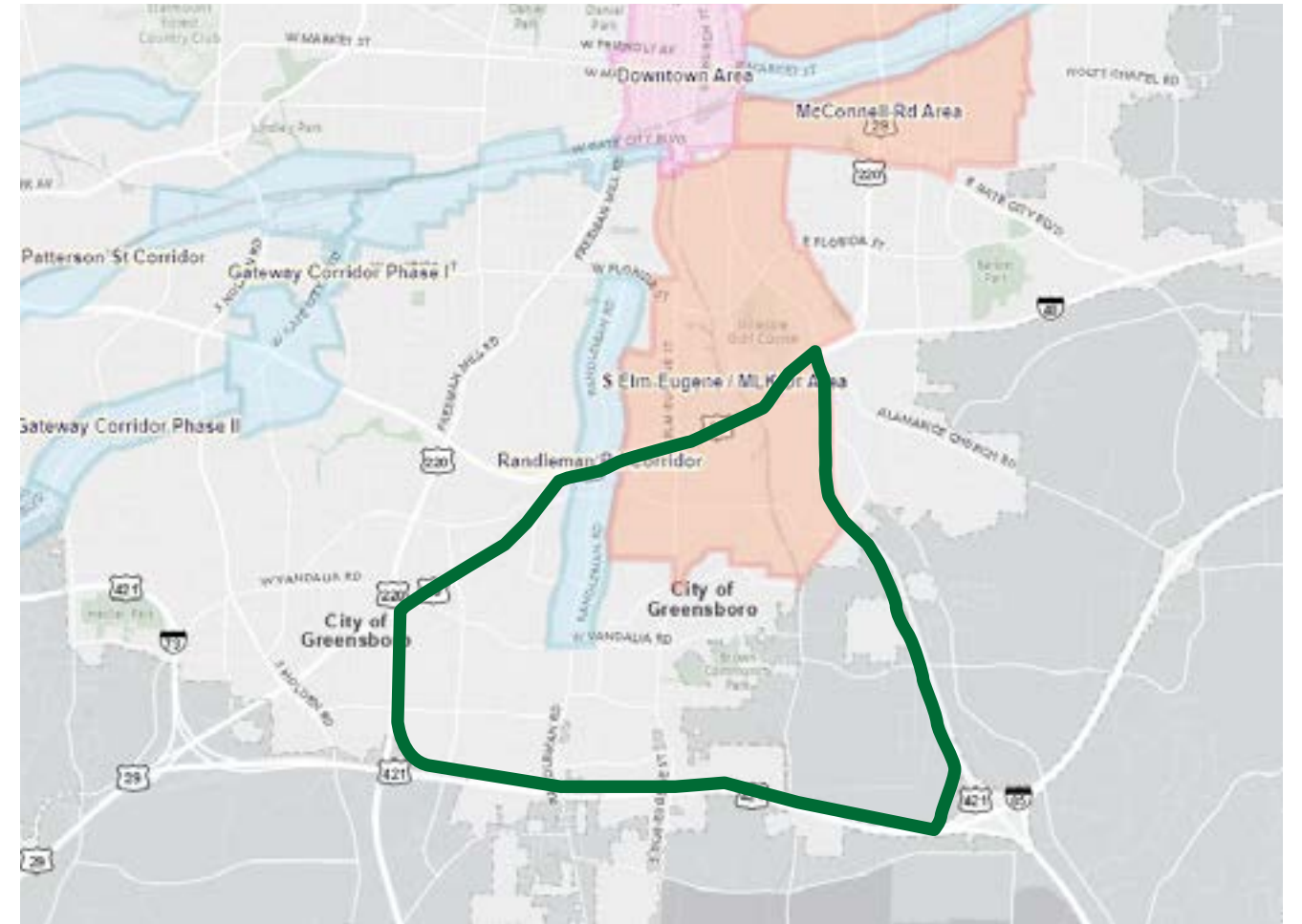


Rezoning that have been approved in or near the Study Area are shown on the map above. They include approximately 500 single family units outside the Study Area, 1,500 multifamily units and 525 mixed residential units. These rezonings are generally clustered at the southern end of the Study Area near the Elmsley Drive retail center and the eastern side near US 421 S and Edgemont Road.

Potential Housing Units - Land Use

- Single-family Residential
- Multi-family Residential
- Mixed Residential

Reinvestment Areas and Corridors



These areas were identified in *Connections 2025* for public and private investment. The Urban Development Investment Guidelines that the City uses to promote infill and redevelopment to create more economically and socially vibrant communities may be applicable in these areas.

Timeline of Key Planning Efforts

PRE-2019

Bi-Ped Plan (2015)
Randleman Road Corridor Study - Phase 1 (2016)

2019

Plan2Play: Greensboro Parks & Recreation Master Plan

*Start of COVID-19 Global Pandemic

2020

GSO2040 Comprehensive Plan
Housing GSO: Creating Opportunities to Build a Better Community

2022

Southeast Greensboro Coalition White Paper

2023

Southeast Greensboro Community Conversations
Southeast Greensboro Coalition Response to Community Conversations

2024

Randleman Road Corridor Study - Phase 2
Highway 421 Land Use Study
Guilford County Comprehensive Plan



ADDITIONAL PLANS, STUDIES AND DOCUMENTS REVIEWED

- Unified Planning Work Program FY 24-25
- Growing Greensboro – Potential Housing Units 2021-2023
- BiPed Plan A6 MPO Resolution
- Randleman Road Corridor Study (Ph 1 & 2)
- South Greensboro Coalition Proposal and Response to Community Conversations
- Program of Projects FY 2023
- Randleman Road Accomplishments Memo
- East Vandalia Roadway Improvements
- BiPed Plan and Amendment Update 2018

Common Themes, Issues, and Opportunities Across Documents Reviewed

COMMON THEMES: REFLECT COMMUNITY CONCERNS AND VISION AS DOCUMENTED IN PREVIOUS PLANNING EFFORTS

Mixed-Use Development

- Encourage compact, mixed-use, walkable development
- Create consistent patterns in development through design guidelines and/or the land development ordinance
- Promote high-quality infill development and foster sustainable development practices

Economic Growth

- Support local and small businesses; Attract new businesses and industries to stimulate job creation and economic development
- Target workforce development activities to enhance local employment opportunities
- Encourage different types of businesses (e.g. healthy foods) to avoid over-concentration of Dollar General-type businesses
- Incremental development and renovation of existing businesses

Beautification and Maintenance

- Enhanced landscaping and/or public art at key gateways; program for maintenance
- Interchanges should be gateways, also key internal intersections and other gateway opportunities should be enhanced

Incorporate Green Infrastructure

- Mitigate urban heat island effect and stormwater runoff
- Upgrade public ROW to more attractive and sustainable standards
- Private development standards – parking areas to use more green infrastructure and sustainable standards

Transportation and Connectivity Infrastructure

- Defined street standards; enhanced with more pedestrian amenities (e.g. street trees, lighting)
- Improve multi-modal options, greenways, connections and pedestrian safety

- Enhance bicycle access with dedicated facilities and improvements.
- Evaluate and repair roads, bridges, and public utilities.

Transportation Safety

- Randleman/Vandalia intersection – relatively high crash density
- Randleman Road – other areas, growing crash density
- Growing crash densities in “Activity Centers” place types
- Mobility safety issues occurring in activity center/retail/entertainment areas

Public Transit

- Randleman Rd and S Elm-Eugene have highest transit ridership in city
- Enhance the efficiency and reliability of public transit
- Seamlessly combining public transit with active transportation networks
- Upgrade bus stops and planning routes for better service.

Equitable Access and Social Equity

- Promotion of social equity through inclusive planning, inclusive investment strategies, equitable access to transportation, development benefits, resources, and opportunities, particularly to underserved and minority communities.

Equitable Zoning & Development

- Several industrial land uses adjacent to residential land uses causes nuisance issues and negative externalities
- Tractor trailer and delivery truck nuisances (engine noise, air brake noise), causing lower quality of life, including lack of sleep
- Higher burden on residents, higher chance of residential turnover and displacement
- Concern about higher negative environmental health impacts (air quality, water quality, noise, heat)

COMMON THEMES (CONTINUED)

Active Community Involvement

- Encourage active collaboration with community members and stakeholders in planning processes.
- Build trust between the community and the City through consistent and transparent actions.

Enhanced Safety Measures

- Improved lighting and increased community engagement in high crime areas.
- Community policing efforts to build trust between residents and law enforcement.

Public Awareness and Preparedness

- Campaigns to educate the public about safety measures and emergency preparedness.

Natural and Cultural Resources

- Need for more park space in Study Area; perhaps larger regional park
- Capitalize on natural amenities like South Buffalo Creek and Ryan Creek for ecological and recreational benefits.
- Create connections to significant resources such as churches, schools, commercial centers, and parks to foster a cohesive and sustainable community.

Community Health and Wellness

- Programs focusing on preventive care and overall wellness for community members
- Promote active living through well-connected parks and recreational facilities

Sustainable Practices and Policies

- Emphasize sustainable development to protect and enhance green spaces
- Implement eco-friendly policies and infrastructure improvements to support long-term environmental health

Green Spaces and Urban Tree Canopy

- Implement greenways and connections to promote outdoor activities and environmental stewardship
- Initiatives to expand green space and enhance urban tree canopy

Revitalization Projects

- Specific efforts to revitalize areas like Randleman Road, addressing community concerns and encouraging sustainable development.
- Opportunities for new park spaces and better distribution of recreational facilities to meet community needs

COMMON ISSUES & OPPORTUNITIES: RECOGNIZING OPPORTUNITIES TO ADDRESS COMMUNITY CONCERNS AND VISION

Infrastructure Deficiencies

- Address inadequate pedestrian and transit amenities, facilities, crossings, and connections.
- Update and improve aging public infrastructure, including accessibility features for the elderly and disabled.
- Secure sufficient funding and manage resources effectively for comprehensive infrastructure improvements.

Negative Public Perception

- Develop and implement policies that prioritize public safety in urban planning and development initiatives.
- Integrate safety considerations into all aspects of planning, from infrastructure design to community programs.
- Foster collaboration between law enforcement, the city, and community members to create safer neighborhoods.

Environmental, Public Health and Safety Initiatives

- Implement comprehensive safety action plans in coordination with public health, law enforcement, and educational partners.
- Promote health through active living environments with safe, inviting, and accessible infrastructure.
- Initiatives to protect and restore natural habitats and biodiversity.

Economic Challenges

- Attract new businesses with tax incentives and financial aid.
- Implement support programs for struggling local businesses to prevent closures.
- Develop economic strategies targeting underdeveloped areas to stimulate growth.

Lack of Planning and Coordination Efforts

- Establish an area planning coalition to synchronize efforts and develop a comprehensive plan for the area.
- Ensure multi-stakeholder input in planning processes and regularly review and adjust policies to adapt to changing needs.
- Address environmental concerns by balancing development with sustainability goals and increasing green spaces.

Public-Private Partnerships

- Foster collaborations between public entities and private businesses to support economic growth and infrastructure projects.
- Develop incentives to attract developers and businesses to key areas for strategic investments.

Rezoning Concerns and Development Pressure

- Conduct transparent community consultations on rezoning applications.
- Balance development with the preservation of community character through strategic planning.
- Adjust regulations to manage growth without displacing current residents.

Enhanced Public Transportation and Active Mobility

- Use improved data collection and analysis tools to better understand and meet transportation needs.
- Expand greenways, trails, and pedestrian networks to connect communities and support non-motorized transportation.
- Create a smart traffic management systems to reduce congestion and improve traffic flow.

Community-Centered Development

- Focus on walkable, mixed-use development that enhance neighborhood character and quality of life.
- Leverage existing commercial zoning for mixed-use development
- Encourage zoning changes to support developments that encourage walking and reduce car dependency.
- Zoning, land development ordinance opportunities to improve consistency of setbacks and minimize curb cuts.

Community Conversations and the Southeast Greensboro Coalition (SEGC)

Important Input Carried Forward

A distinct moment in the planning timeline appears to be surrounding two documents from the Southeast Greensboro Coalition and the Community Conversations effort in 2022. Important messages, ideas, input, and other elements to consider in the South Greensboro Area Plan planning process are provided here. (Note: the difference between Southeast Greensboro area of interest and the Study Area boundary of the South Greensboro Area Plan is explored below.)

In Southeast Greensboro, tension arose between residents and rezoning applicants due to differing views on appropriate development, with some residents concerned about the concentration of multi-family housing and the perceived neglect of community needs.

Discussions in Southeast Greensboro have centered on the lack of infrastructure, particularly sidewalks, sound walls, road resurfacing, stormwater management, and parks. Residents highlighted perceived inequities in capital investment compared to affluent areas and expressed strong dissatisfaction with the inadequate parks and recreational amenities in the area.

“The City of Greensboro wanted to find a way forward that respected the concerns of the Southeast Greensboro community within the framework of the Greensboro 2040 Comprehensive Plan (GSO 2040 Comprehensive Plan), which outlines goals for the City as a whole.”

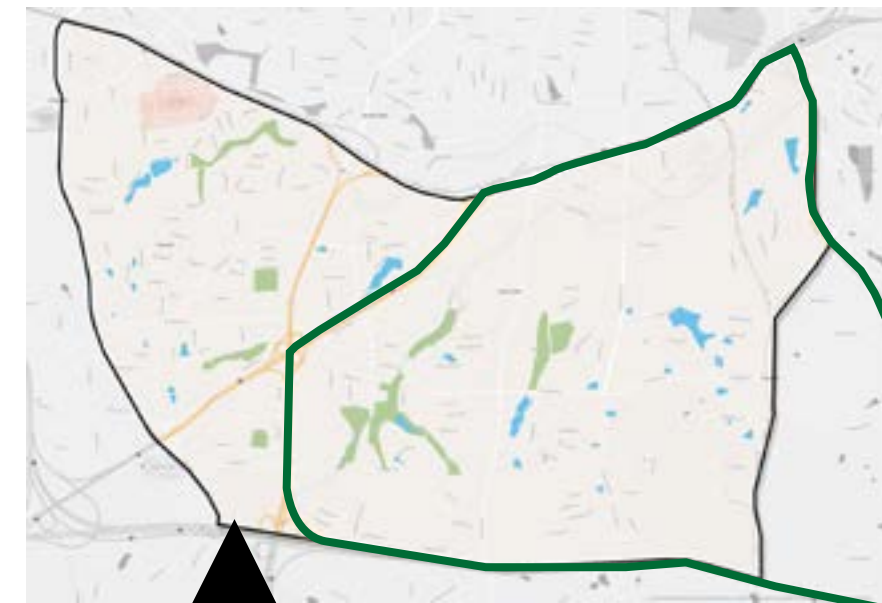


Residents in Southeast Greensboro cited code enforcement and quality of life issues, including the impact of nearby industrial areas, truck routes, and highway noise, as evidence of the City’s lack of concern. They have called for stronger accountability and better communication with the City.

During the Community Conversations of 2022, residents raised concerns about community relations, expressing sentiments of lacking accountability, follow-through, and respect, despite the City having a Division of Community Relations intended to foster positive, long-term relationships and assist with navigating City services.

The community also expressed concerns about aging infrastructure, inequity in public investment, the need for improved shopping options, diminished pride due to litter, and the necessity for stronger City accountability regarding code enforcement and citizen engagement.

“With the communities in SE Greensboro being placed in the middle of two major interstates and three industrial sites, our community has grave concern around being a hot spot with poor air quality, as this is where we raise our families.”



The South Greensboro Area Plan Study Area boundary overlaps, but is different than, the area of interest for the Southeast Greensboro Coalition (SEGC) and the 2022 Community Conversations held by SEGC, the City, and consultant team.

South Greensboro Area Plan Study Area boundary

SEGC and 2022 Community Conversations area of interest

Plan 2 Play (2019) Parks Rec Plan



- Robust community engagement process
- Identified need for 400 acres of park land by 2038
- Facility priorities: paved walking/biking trails, natural trails, indoor fitness, indoor pools, public gardens
- Applicable concepts in enhance, expand, connect the framework
- EXPAND: South Buffalo Drainageway improvements (with illustration of nearby site)
- ENHANCE: Community heart (pgs 110-113)
- Shannon Woods and Shannon Hills meet all criteria on page 113 except "Community Destination"
- Brown Center Park meet all criteria on page 113 except "High Population Density"
- Case Study: Nocho Park and Windsor Park Recreation Center (maybe applicable to Brown)
- CONNECT: Variety of opportunities related to the
- CONNECT portion of the framework, especially along South Buffalo Creek and Ryan Creek. Actions items 7a-d are applicable.

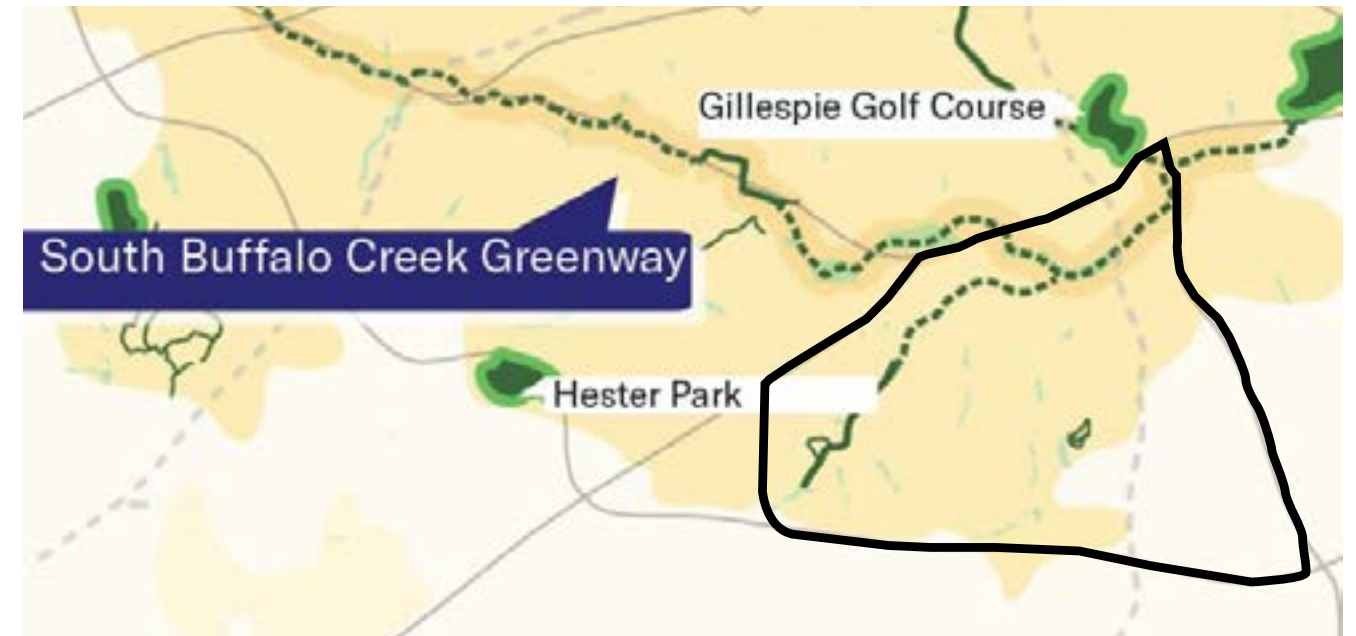


Source: Plan2Play Park & Recreation Master Plan

Plan 2 Play

"Action 4a. Identify Community Hearts based on a common set of principles."

The Study Area contains no planned regional parks. A small segment of the planned citywide urban loop trail is in the northern part of the Study Area (which is mostly industrial). The plan framework shows several neighborhood parks and a "community heart."



Source: Plan2Play Park & Recreation Master Plan

Plan 2 Play

The citywide Urban Loop Trail is located in the northern portion of the Study Area (which is almost entirely industrial and commercial). There are several planned and potential greenways that lead to this segment.

The potential for this segment to be part of the Urban Loop Trail provides an opportunity to rethink the urban fabric around it.



The following pages compare strategies in the Plan 2 Play plan with relevant potential opportunity spaces in the Study Area.



Plan 2 Play Recommendations

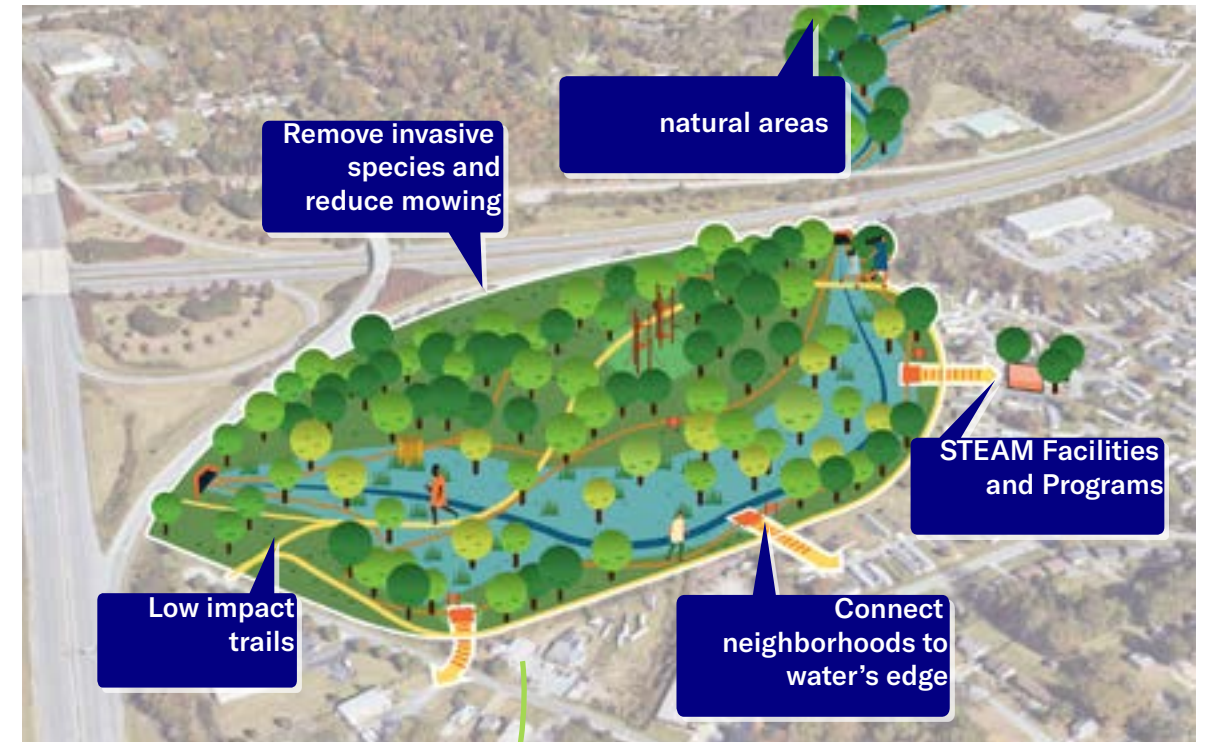
Case Study: Windsor Recreation Center and Nocho Park reimagined as a community heart.

Source: Plan2Play Park & Recreation Master Plan



Applying Plan2Play in South Greensboro

Could Brown Recreation Center be reimagined as a community heart?



Plan 2 Play Recommendations

Source: Plan2Play Park & Recreation Master Plan

Action 3b. Create “green infrastructure” by formally adopting the drainageways and designing them for water resilience. Concept for South Buffalo Drainageway improvements near the Study Area.



Applying Plan2Play in South Greensboro

Much of the Study Area contains a system of streams and drainageways that are tributaries of the South Buffalo Creek Drainageway. Many of these green corridors are locations for existing or planned greenways.

Plan 2 Play Recommendations

Some action items from Plan2Play relate directly to parks within the South Greensboro Study Area. For example, Greenhaven Park (below) on Lynhaven Drive in the western portion of the study area has a greenway segment that connects to other parks in neighborhoods to the south. It is also planned to connect to areas to the north and to planned greenways along Buffalo Creek. Related Plan2Play action items are:

Action 7a. Complete a city-wide outer “Community Loop” trail that connects to the region and the Downtown Greenway.

Action 7b. Increase density and diversity of trails within Parks.

Action 7c. Connect trails into the overall multi-modal system and link plans with other planning efforts to seek federal, state, and local funding.

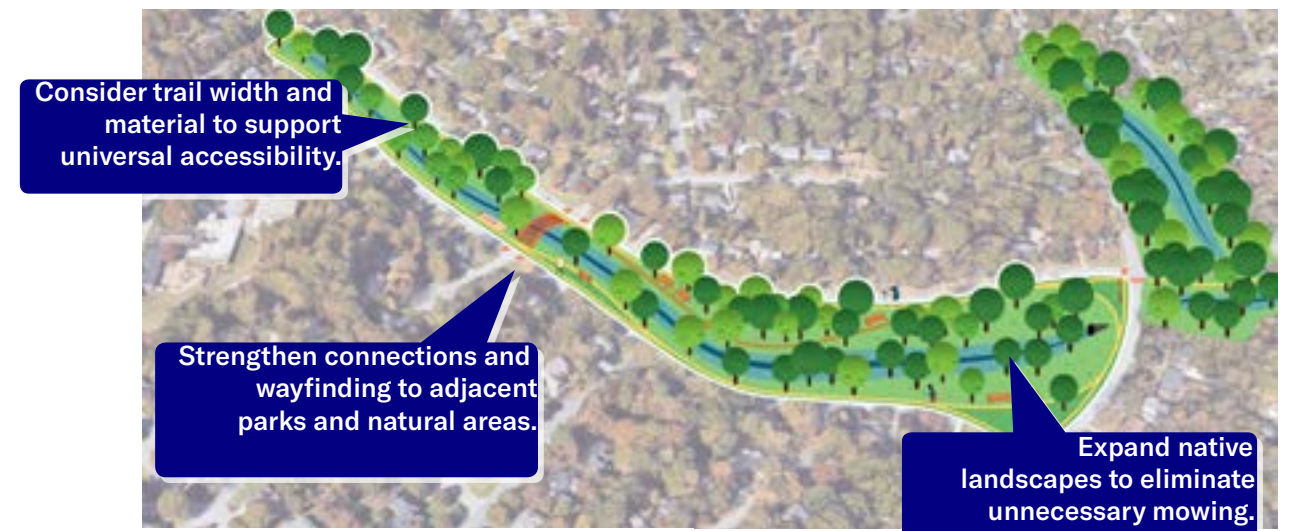
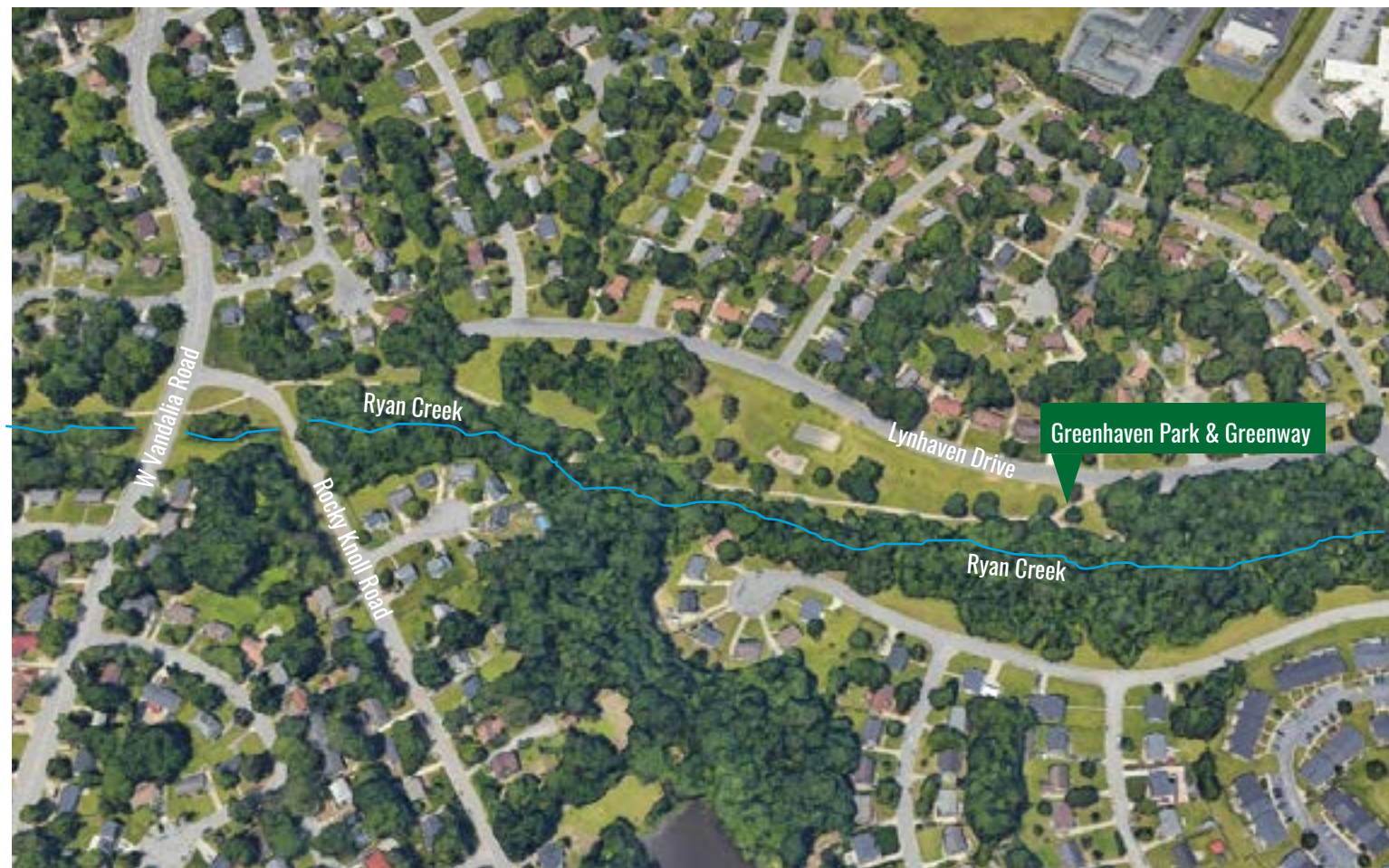
Action 7d. Build on the existing watershed trail network and extend the blueway trail network (paddling trails).

Applying Plan2Play in South Greensboro

Images show South Buffalo Creek running through the northern commercial portion of the Study Area (right) and Ryan Creek with a greenway traversing through a residential area in the west portion of the Study Area (opposite).



These linear spaces should be explored for enhancement as recommended in the Plan2Play action items and as shown in the graphic below.



Source: Plan2Play Park & Recreation Master Plan

Transportation

TRANSPORTATION

Overview

The Study Area is surrounded by major freeways, limiting points of connection to surrounding parts of Greensboro while at the same time providing multiple regional access points. School zones cross major interstate barriers which prevent safe and comfortable crossings for parents and students. The interchange at Randleman Road is under consideration for closure as this section of the highway does not meet modern standards for separation between exits. If a closure does occur, it may impact future traffic routing, including truck traffic, away from Randleman Road.

The consulting transportation team has also observed a lack of bicycle and pedestrian connectivity within the Study Area.

- North-South routes are not equipped with bicycle facilities
- Creek Ridge Road / Industrial Ave has bicycle facilities (painted bike lanes) from Randleman Road to Manufacturers Road, where they transition to sharrows
- Main roads typically have back-of-curb sidewalks (minimum width)
- Pedestrian crossings are limited to main, signalized intersections
- Pedestrian markings are often faded or non-existent with standard (two-stripe) crosswalk used as opposed to hi-visibility crosswalk striping
- Hi-visibility ladder style crosswalks are used on Gate City Blvd, which, like Randleman Road is maintained by NCDOT

Of note, Greensboro's 2024 GOBORO Transit Plan recommends improving bus frequencies on Randleman Road to every 15 minutes, as compared to every 30 minutes currently. This corridor would become a high frequency transit corridor which may change commercial and residential land use expectations as well as increase the need for safe and convenient access to transit stops for bicyclists and pedestrians.

Census Tracts in the Study Area rank in the 92nd percentile for Transportation Safety (Disadvantaged). Tracts rank in the 67th percentile for Transportation Cost Burden (Disadvantaged). The average South Greensboro household spends 15.61% of their household income on transportation.

Posted speed limits in the Study Area are as follows, but the community has reported speeding and aggressive driving behavior, especially on Randleman Road:

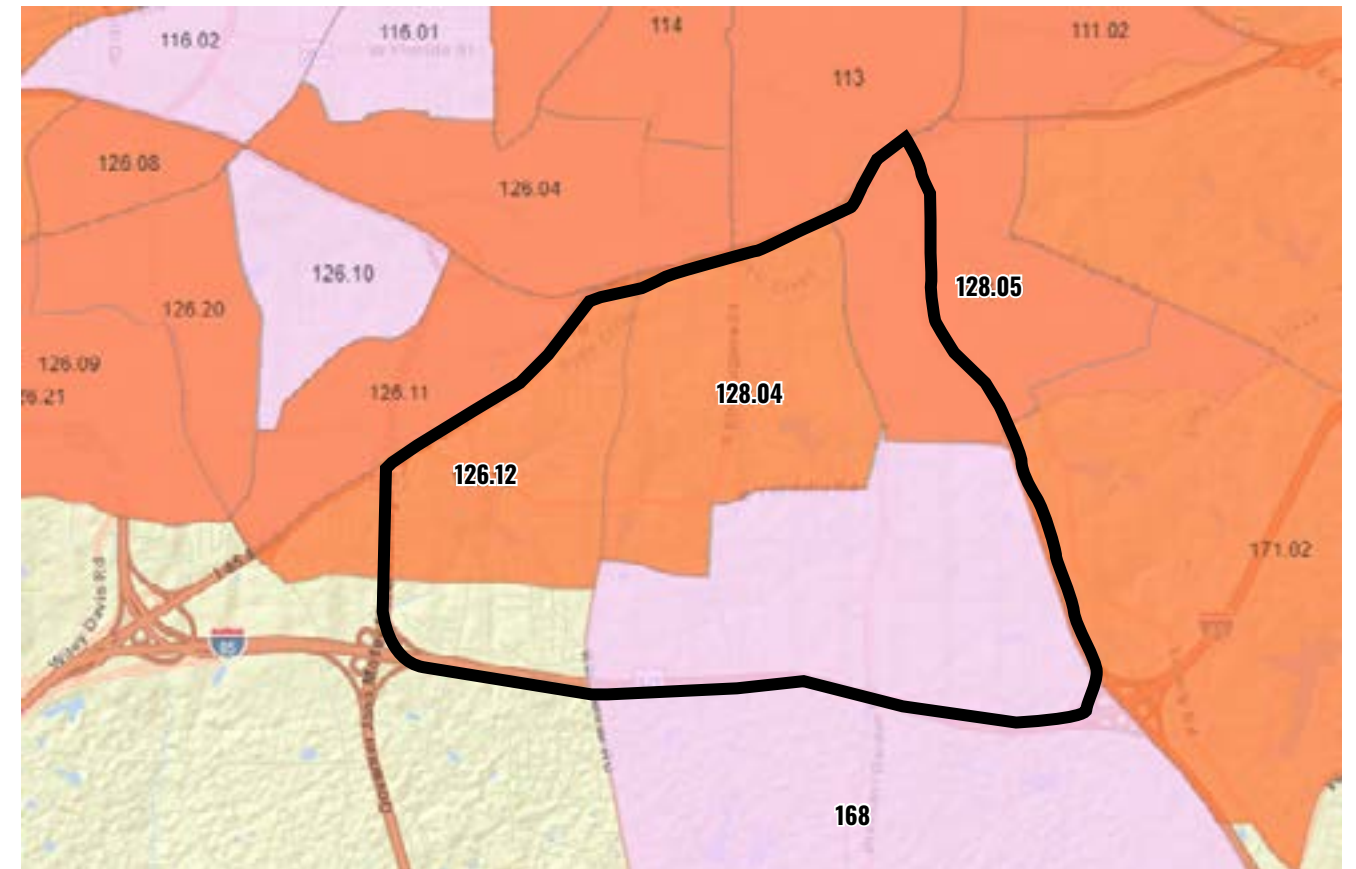
- Rehobeth Church Road: 35 mph to 40 mph
- Randleman Road: 35 mph
- Elm-Eugene: 35 mph
- Riverdale Road: 45 mph
- Pleasant Garden Road: 45 mph

Commute Profile		
	Study Area	City
ACS Workers Age 16+	7,944	142,154
Drove Alone to Work	83.1%	75.7%
Walk	1.9%	2.4%
Carpool	5.3%	8.3%
Took Public Transportation	0.8%	2.2%
Bike	0.0%	0.2%

Source: U.S. Census Bureau, 2018-2022 American Community Survey

KEY POINTS

- **Two Pedestrian Fatalities within the Study Area 2017-2021**
- **Lack of street connectivity throughout study area**
- **Lack of sidewalks throughout**
- **Pedestrian crossings are limited to main, signalized intersections**
- **Planned high frequency transit on Randleman may benefit commercial and residential areas along the route**
- **Freeway interchange at Randleman will be studied in the next five years and redesign or removal are options**



Source: U.S. DOT Areas of Persistent Poverty & Historically Disadvantaged Communities Mapping Tool

Disadvantaged Census Tracts (2010 Census)

- Tract 128.05
- Tract 168

Persistent Poverty Census Tract (2020 Census)

- Tract 126.12
- Tract 128.04
- Tract 128.05

	At-Risk Populations	
	Study Area	City
Car-Free Households	464 of 6,935 (6%)	9,099 of 125,761 (7%)
Households with a Disability	1,552 of 6,935 (22%)	26,857 of 125,761 (21%)
Households Below the Poverty Level	1,258 of 6,935 (18%)	20,693 of 125,761 (17%)
Population 65+	2,330 of 16,045 (14.5%)	48,996 of 305,047 (16%)
Population under 18	3,639 of 16,045 (23%)	61,155 of 305,047 (20%)

Source: U.S. Census Bureau, 2018-2022 American Community Survey

Mobility

Sidewalks and bicycle facilities are important for equitable mobility and to provide options for people to move around easily without a car.

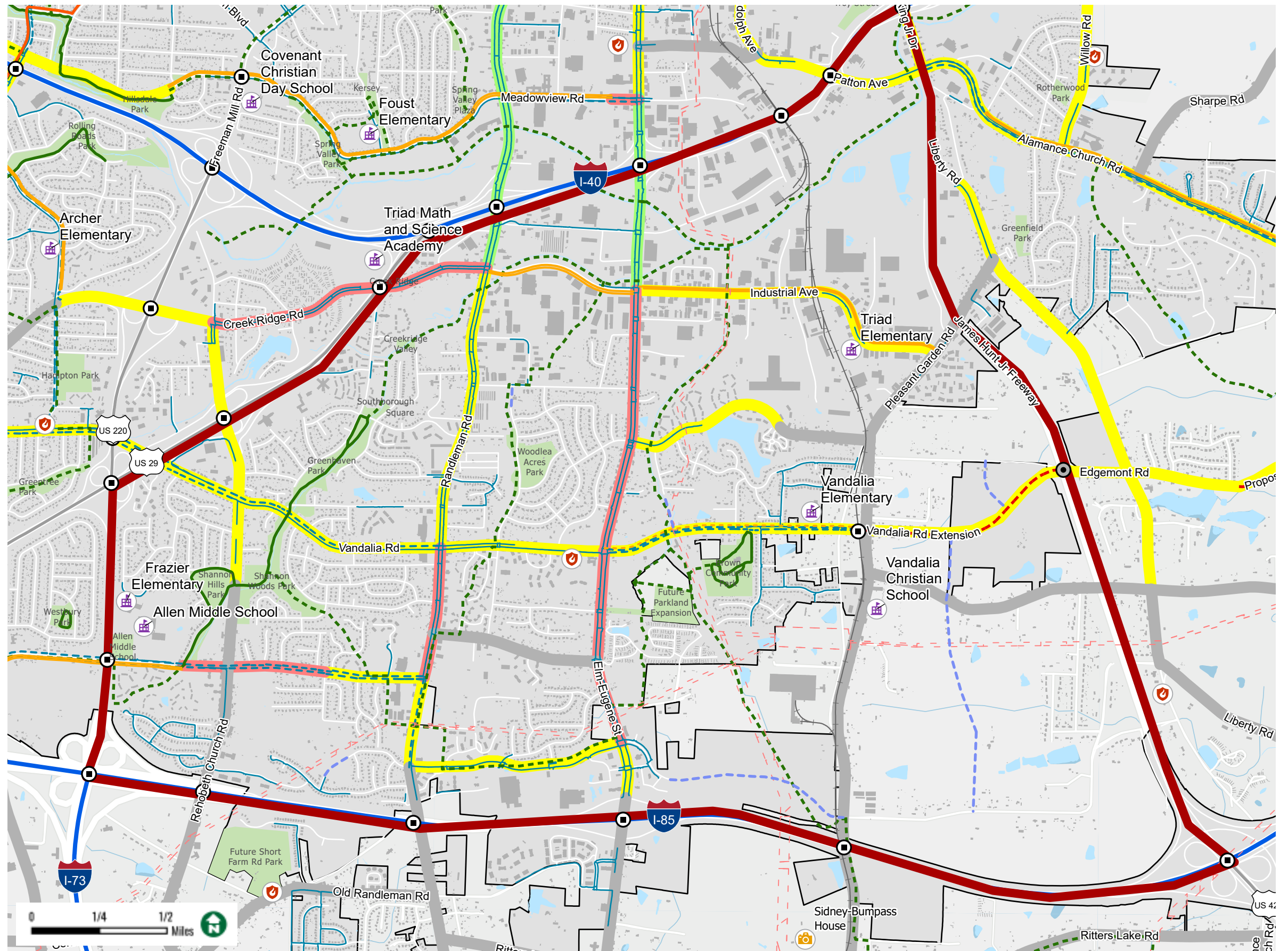
Sidewalks are generally consistently present on both sides of Randleman Road and S Elm Eugene Street throughout the Study Area, with the exception of portions of Randleman south of Glendale Road. Vandalia Road has intermittent sidewalks. Most neighborhoods adjacent to these corridors do not have sidewalks at all.

Bicycle facilities are limited to Industrial Drive/Creek Ridge Road between Randleman Road and Pleasant Garden Road and a small segment on Glendale Road west of the Greenhaven Greenway.



Sidewalk segment and buffered bicycle lane on Creek Ridge Road west of S Elm-Eugene Street

- Sidewalks
- GDOT Bike Facility
- BiPed Recommended Bike Lanes (2018)**
- Bicycle Lane
- Protected Lane
- Sharrows
- Shoulders
- - - Planned Sidewalk Projects
- - - Recommended Greenway
- ~ Trails and Greenways



Thoroughfare and Collector Street Plan

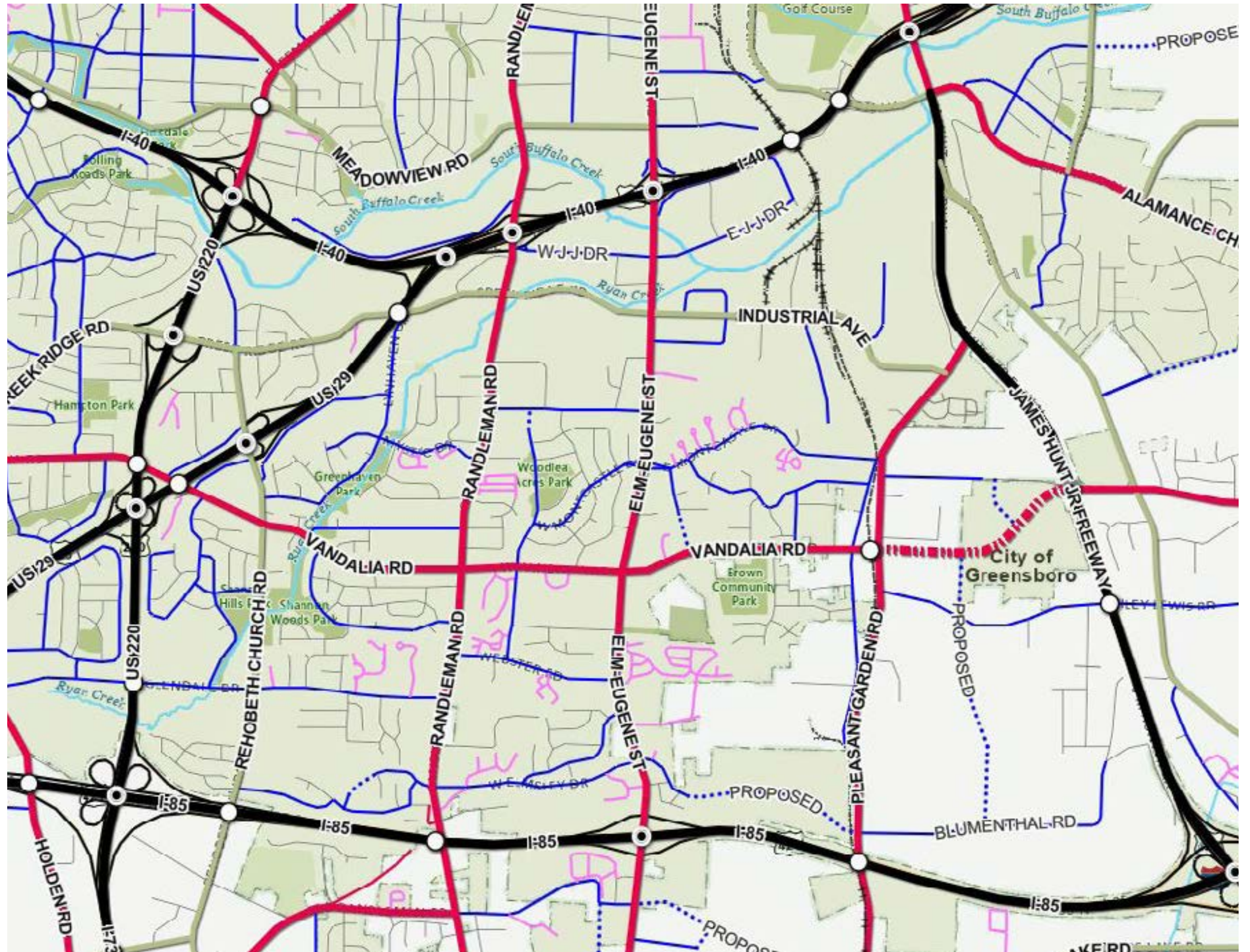
This plan shows existing facilities as well as street connections planned for the future as shown in previous plans. The feasibility and timeline of these planned connections is unknown.

Upcoming Transportation Projects

- Mobility Plan/Transit Plan Update (On-Going)
- Safety Action Plan to Implement Vision Zero
- Urban Street Design Guidelines and Requirements for Complete Street Development modeled on Charlotte, NC and NCDOT Guidelines
- Better Bus Stop Amenity Implementation Plan
- Long Range Transportation Plan update to reflect Comprehensive Plan
- E. Vandalia Road Redesign from S Elm-Eugene Street to Pleasant Garden Road; widening from 2 lanes to 3 lanes and adding bicycle lanes and sidewalks

- Existing Interchange
- Proposed Interchange
- Existing Grade Separation
- Proposed Grade Separation
- Existing Freeway
- Proposed Freeway
- Existing Major Thoroughfare
- Proposed Freeway
- Existing Minor Thoroughfare
- Proposed Minor Thoroughfare
- Existing Collector
- Proposed Collector

Source: GUAMPO Thoroughfare & Collector Street Plan



Transit

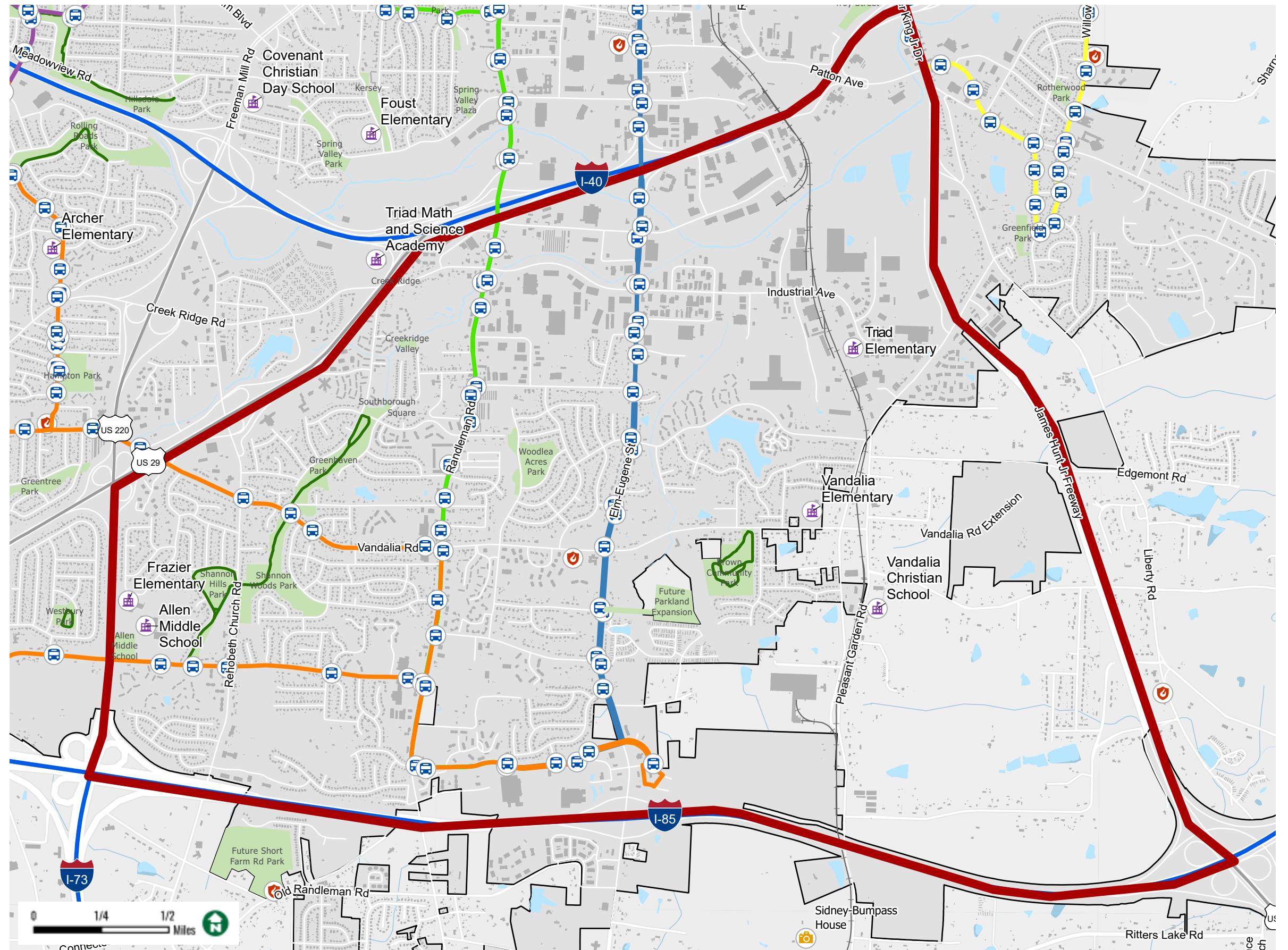
- **Route 12 (1/2 Hour Frequency):** South Elm-Eugene Street - Serving Union Square, Guilford Child Development Center, Elmsley Square Shopping Center, J. Edward Kitchen Operations Center, and GTA Operations Facility and Administrative Offices.
- **Route 12A (1 Hour Frequency):** Southtown Connector - Serving Benchmark Square, Hemphill Branch Library, Holden Crossing, Elmsley Square (Walmart), Four Seasons Town Centre, Wet N' Wild Emerald Pointe, and connections with GTA Routes 2, 12 and 13. This is the only GTA route that does not serve the J. Douglas Galyon Depot.
- **Route 13: Randleman Road (1/2 Hour Frequency) -** Serving Carolina Theatre, Goodwill Industries, Greensboro Urban Ministries, Greensboro College Sports Complex, Salvation Army Boys & Girls Clubs, Warnersville Recreation Center, Spring Valley Shopping Center, Benchmark Square, Elmsley Square, and Walmart.

Mobility Plan / Transit Plan Update (Draft)

- 15-minute service proposed on Randleman
- 30-minute service proposed on Elm/Eugene
- 60-minute service traveling east-west through Study Area on major routes

Bus Route

- 11
- 12
- 13
- 2
- 312
- 4
-  Bus Stops



Crashes and Average Traffic

Crashes in the Study Area are concentrated at:

- Randleman Road and Vandalia Road (most mentioned Intersection)
- Randleman Road and Creek Ridge Road
- S Elm-Eugene Street and Creek Ridge Road
- S Elm-Eugene Street and Elmsley Drive
- Pleasant Garden Road and Riverdale Road

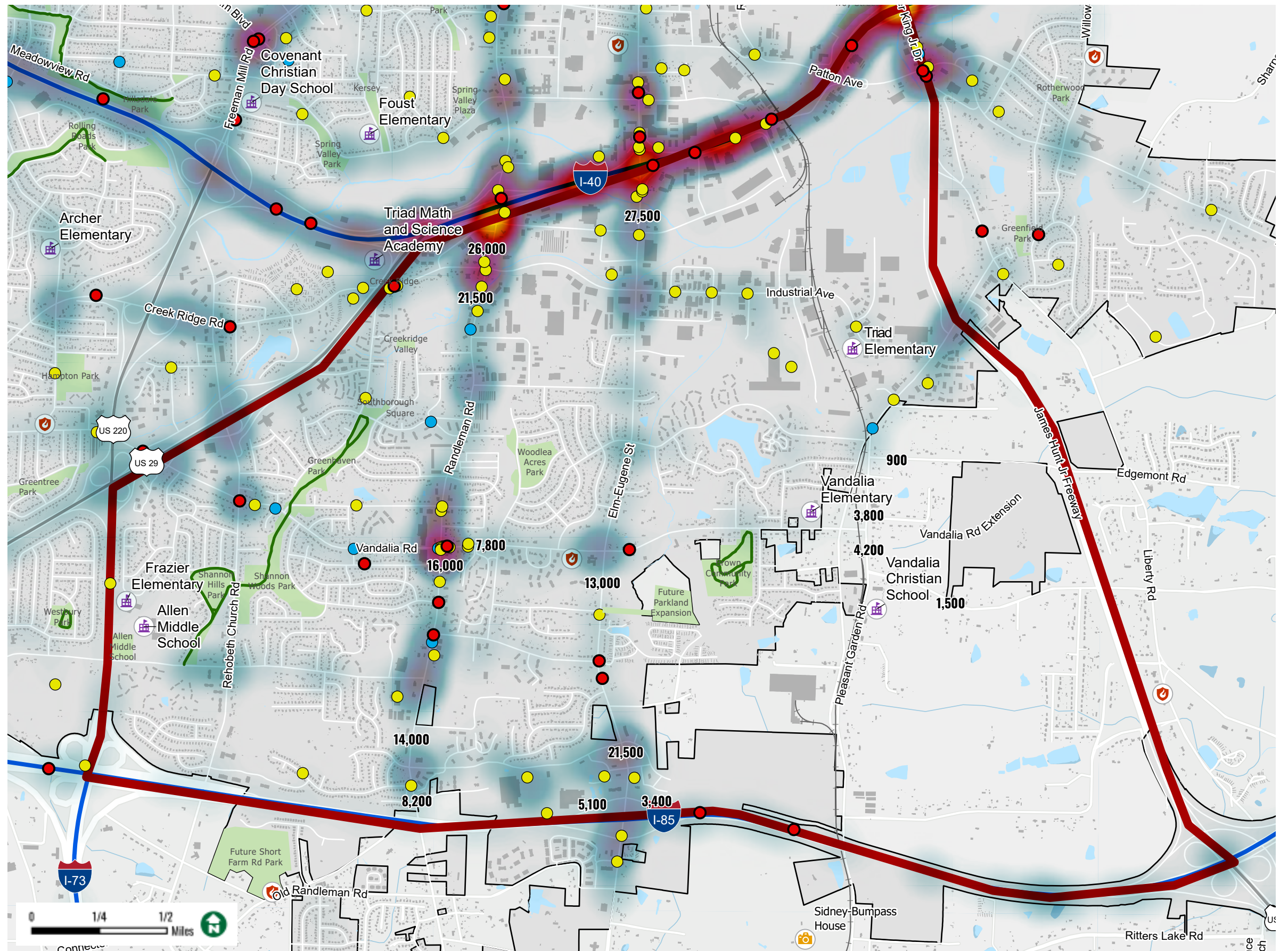
Fatal crashes are generally clustered along Vandalia Road from S Elm-Eugene Street to Rehobeth Church Road, and along portions of Randleman Road and S Elm-Eugene Streets south of Vandalia Road. These areas are predominately within developed residential neighborhoods. Some pedestrian and bicycle crashes follow similar patterns as fatal crashes, but there are clusters of pedestrian crashes in the Elmsley Drive retail center and along Creek Ridge Road/Industrial Drive in the industrial district.

Annual Average Daily Traffic (AADT) 2021

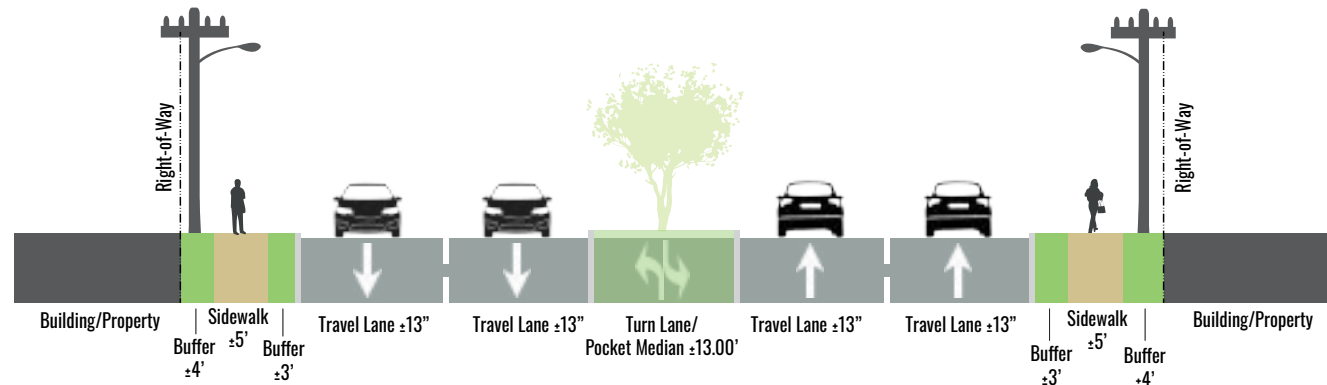
XX,XXX

Crashes (2017-2021)

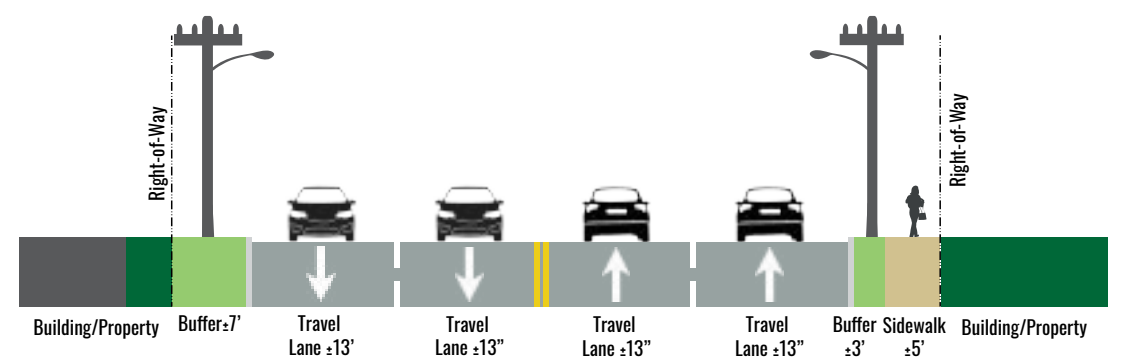
- Sparse
- Dense
- Involving Bicyclist
- Involving Pedestrian
- Involving Fatality
- Planned sidewalk projects
- Planned bicycle projects



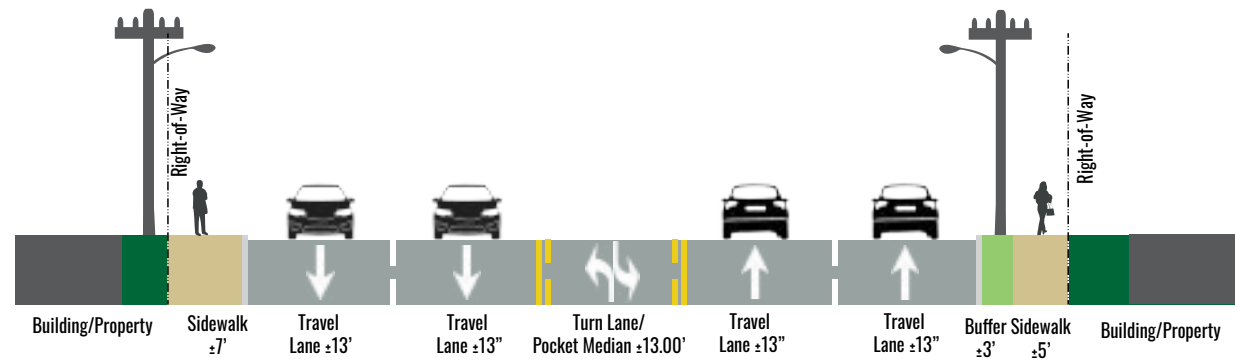
TYPICAL EXISTING CROSS SECTIONS OF MAIN CORRIDORS



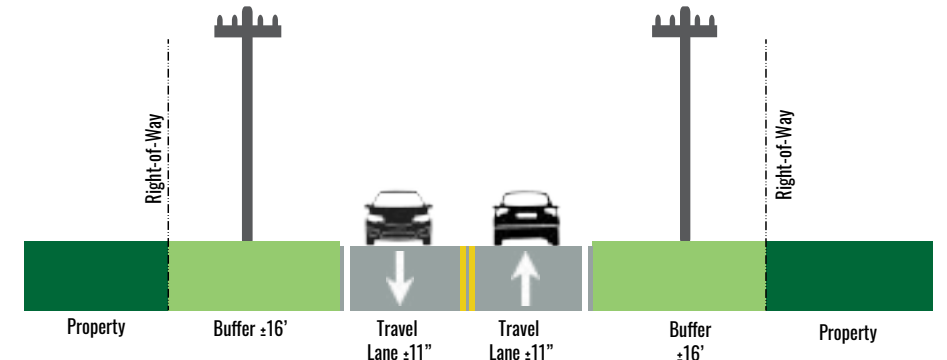
1 S. Elm- Eugene St.- R.O.W. 90 Feet
 Range: 150' from W Elmsley Dr to I-85
 Range: 70' from Corliss St to Blue Bell Rd



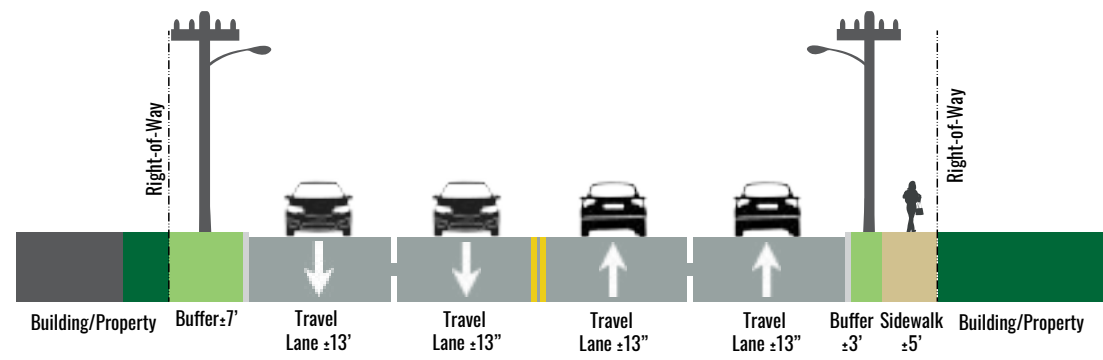
3 W. Vandalia Rd.- R.O.W. 68 Feet
 Range: 50' from Lakfield Dr. to S. Elm-Eugene St.



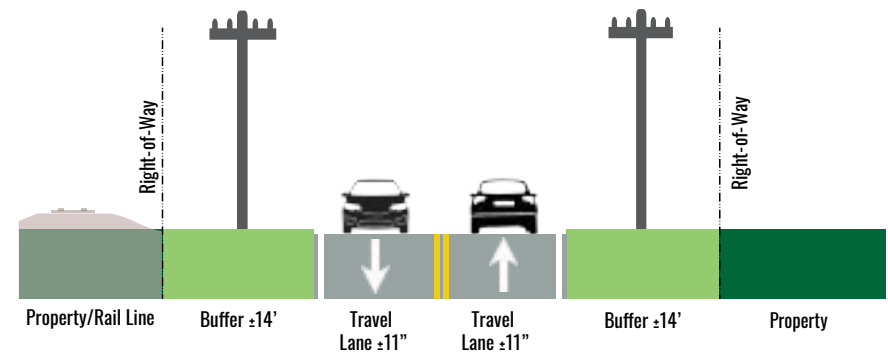
2 Randleman Rd.- R.O.W. 80 Feet State maintained



4 E. Vandalia Rd.- R.O.W. 55 Feet



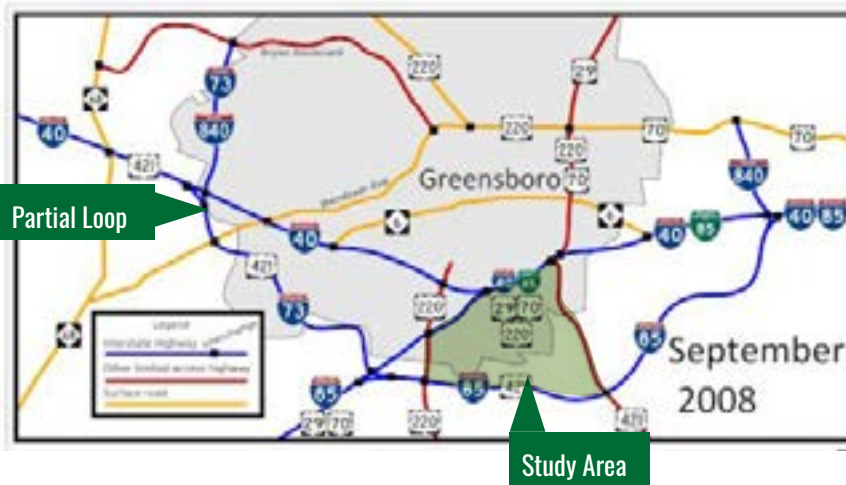
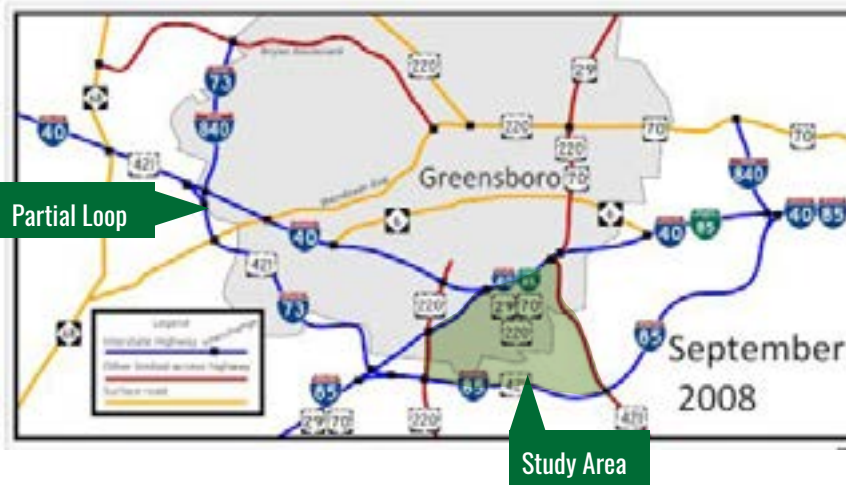
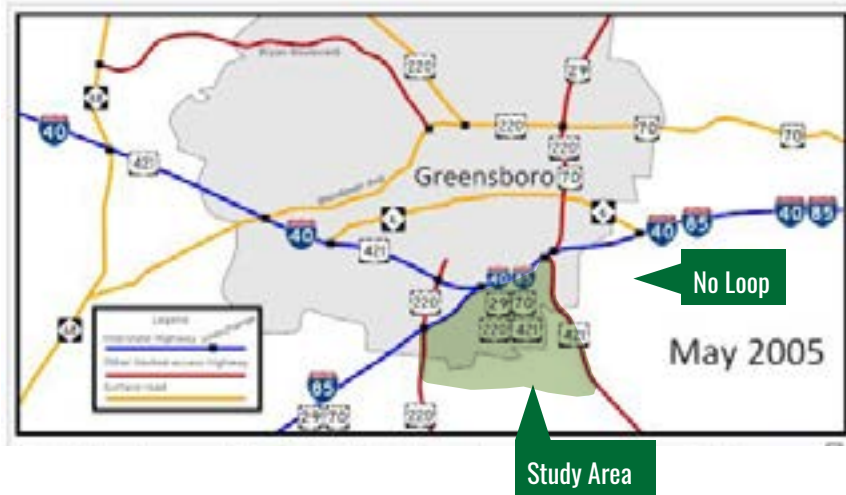
3 W. Vandalia Rd.- R.O.W. 68 Feet
 Range: 50' from Lakfield Dr. to S. Elm-Eugene St.



5 Pleasant Garden Rd.- R.O.W. 50 Feet

URBAN LOOP

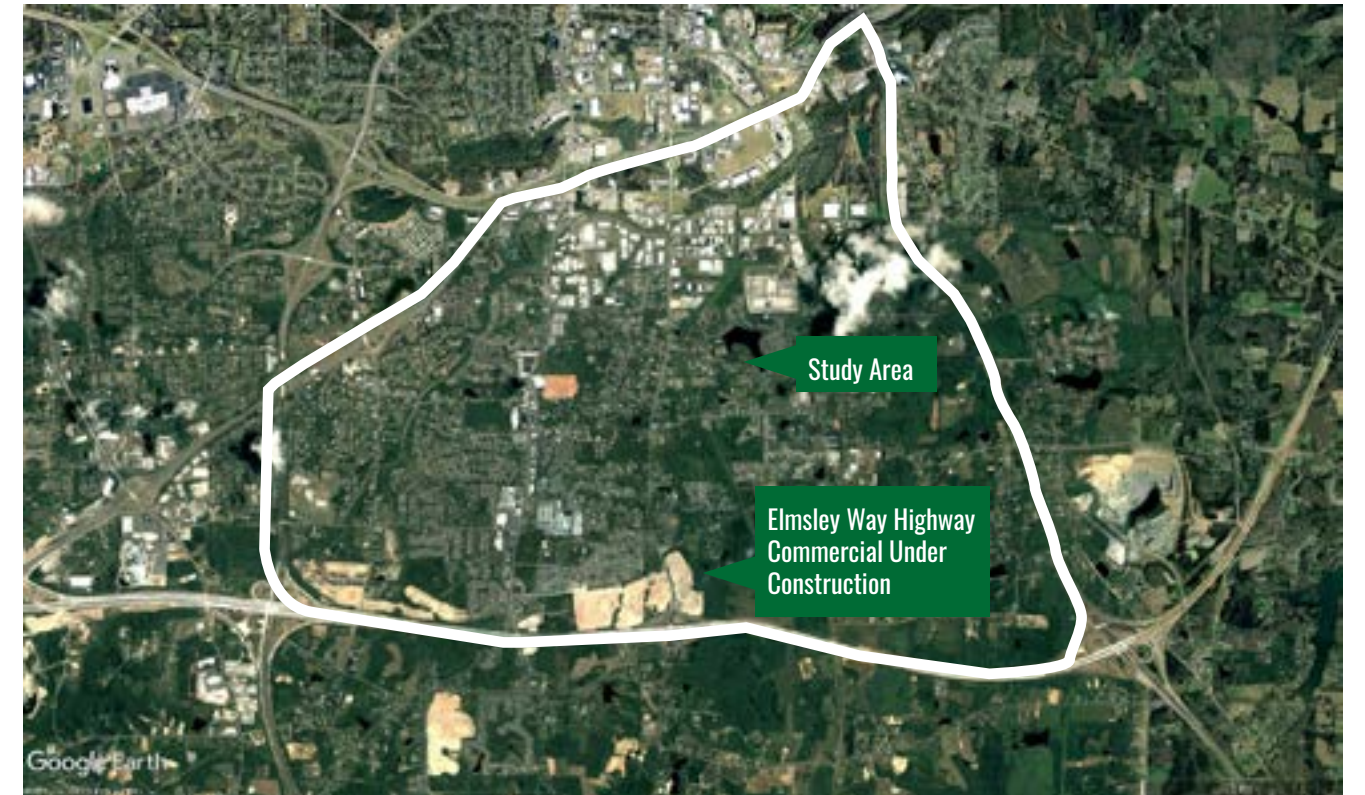
The completion of the Urban Loop is a noteworthy major infrastructure project affecting the Study Area. This freeway provides regional access and defines the southern edge of the Study Area. At this point, the Study Area is effectively surrounded by major freeways. The Urban Loop catalyzed development and connection in the relatively undeveloped and disconnected southern portion of the Study Area.



Source of Images: https://en.wikipedia.org/wiki/Greensboro_Urban_Loop



Feb 1993 - No Loop



June 2004 - With Partial Loop

RANDLEMAN ROAD CORRIDOR STUDY

Overview

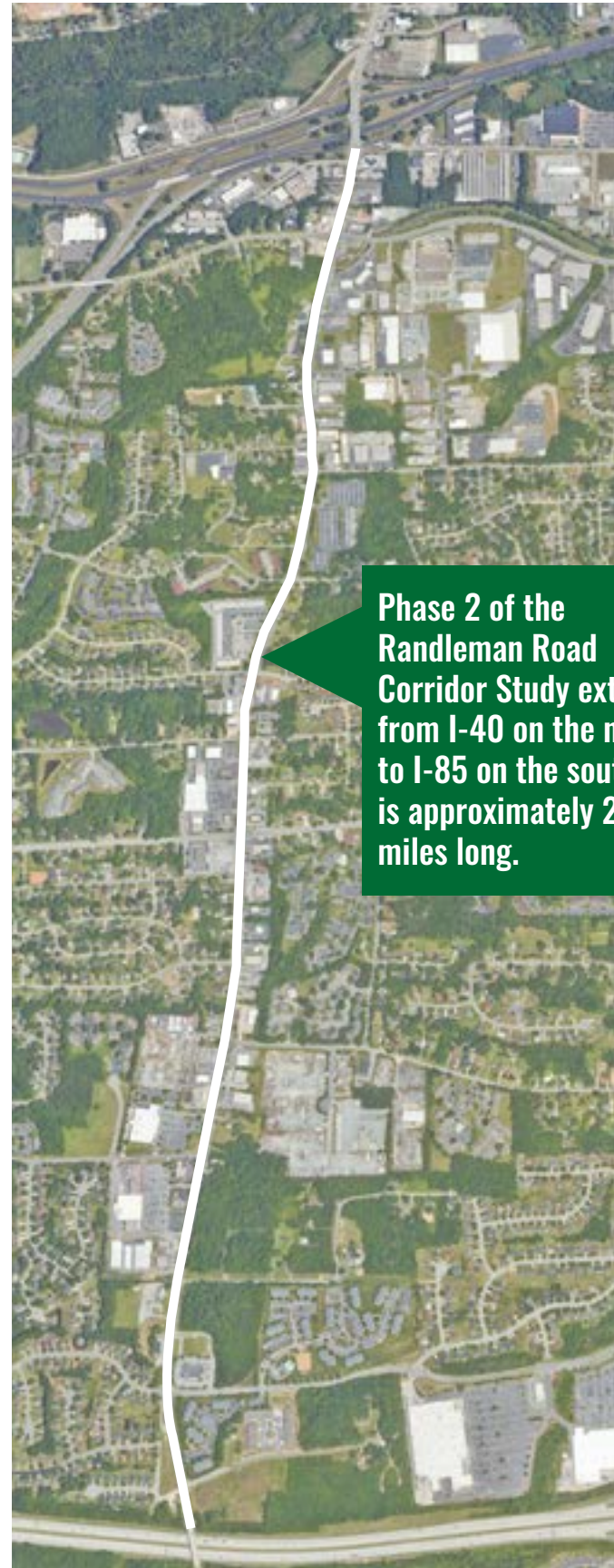
The purpose of this set of studies conducted by the City of Greensboro has been to develop a shared vision for the Randleman Road corridor that can be used by both public and private entities to guide future growth decisions. The policies, tools and strategies identified in the plan will be used by the City to guide implementation activities that support stability in the commercial corridor.

Phase 1

This portion of the corridor is located north of the Study Area and north of I-40. It was studied first and the report was completed in 2016.

Phase 2

This portion of the corridor is currently under study by the City of Greensboro. Existing Conditions have been analyzed and early goals and recommendation have been identified through public engagement.



DRAFT RECOMMENDATIONS FROM THE PHASE 2 RANDLEMAN ROAD CORRIDOR STUDY

WELL-BEING



Foster Sense of Place

Identify areas to implement beautification efforts through decorative plantings and dedication of open space that can be used for City programming.

Incorporate and encourage public art opportunities within new developments or on City-owned land.

Develop wayfinding and signage infrastructure that directs visitors and residents to historical resources, prominent businesses and recreational space.

Prioritize Public Health

Working with public health partners in the local area to explore the possibility of introducing more health institutions to Randleman Road

Connect homeless communities with locally available resources by expanding the service area and working with commercial property owners.

Coordinate with the Guilford County Division of Public Health to ensure temporary outdoor vendors adhere to applicable food safety guidelines.

Connect Resources

Increase the use of the Neighborhood Toolkit Program to help property owners maintain their property and to reduce the impact and duration of community nuisances.

Educate commercial property owners about the Vehicle Release Form for junked and abandoned vehicles within commercial parking lots.

SAFETY



Crime Reduction

Install license plate readers in targeted areas identified through a comprehensive crime rate analysis.

Work more closely with private security firms along the corridor for enhanced communication sharing.

Work with private landowners to install signage on parcels after a Trespassing Letter of Intent has been issued.

Peace of Mind

Install additional LED street lights in areas without sufficient coverage. Additionally, work with private parking lot owners to help brighten up particularly dark parking lots.

Increase the safety of the road, for both motorists and pedestrians, through implementing Vision Zero recommendations and strategies in strategic locations

DRAFT RECOMMENDATIONS FROM THE PHASE 2 RANDLEMAN ROAD CORRIDOR STUDY

MODERNIZATION



Enhance Enforcement

Use the new Code Compliance software to track and enforce code violations by repeat offenders.

Encourage residents to better use compliance services through in person events and seminars, and distribution of shareable resources (on-line, printed, and/or in person).

Increase fines or other financial penalties for illegal truck parking.

Increase city funding and expand programs for demolition of abandoned, dilapidated, and deteriorating commercial properties.

Smart Growth

Encourage new residential and commercial uses that are supportive and complementary of developing employment.

Focus density along the commercial corridor and direct non-single family residential use types into activity centers and limit density east and west of the commercial corridor.

Incentivize Investment

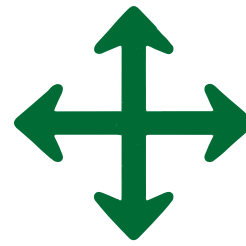
Employ retail recruitment strategies to foster reinvestment on vacant and underutilized land to keep up with changing markets and provide goods and services to area residents.

Promote incremental development in parking lots and underutilized spaces, including short term pop up events and rentable spaces on outparcels.

Connect the corridor's entrepreneurs with business education opportunities, entrepreneurial classes, mentorship programs, and loan acquisition training.

Assist potential investors to assess the feasibility and compatibility of new private development projects.

MOVEMENT



Form and Function

Investigate enhancing City rights-of-way to modernize and provide aesthetically pleasing elements such as plantings, public art, and wayfinding tools. (Vandalia, Glendale, and Creekridge intersections)

Implement treatments on selected major intersections along the corridor, and evaluate intersection operation with traffic signal timing, bike and pedestrian facilities installations, and plantings. (Vandalia, Glendale, and Creekridge intersections)

Accessible Mobility

Enhance the public transit stops along the corridor, including the addition of sheltered stops, ADA accessibility improvements, and the addition of uses at bus stops like rideshare pickups and micro-mobility vehicles.

Improve bicycle accommodations, and where feasible, widen sidewalks to accommodate bicycles and/or implement bicycle lanes as future conditions permit and the community supports the change.

BIPED PLAN (2015) & AMENDMENT (2018)

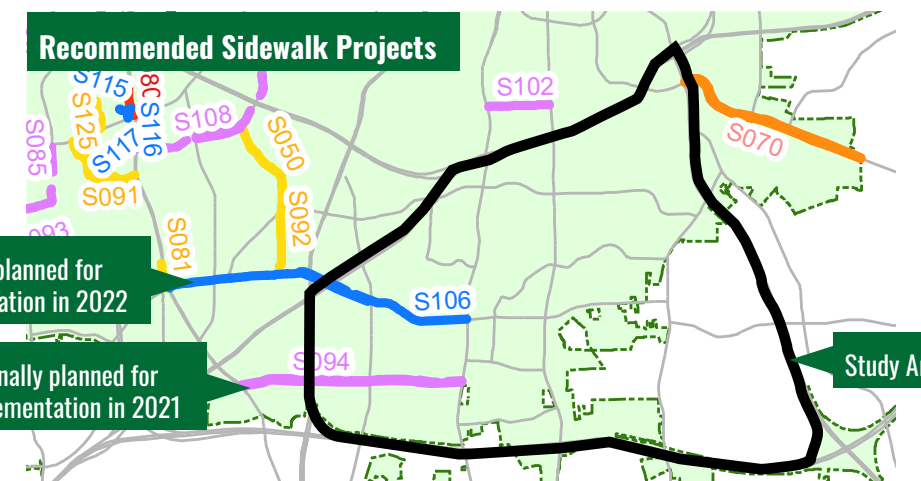
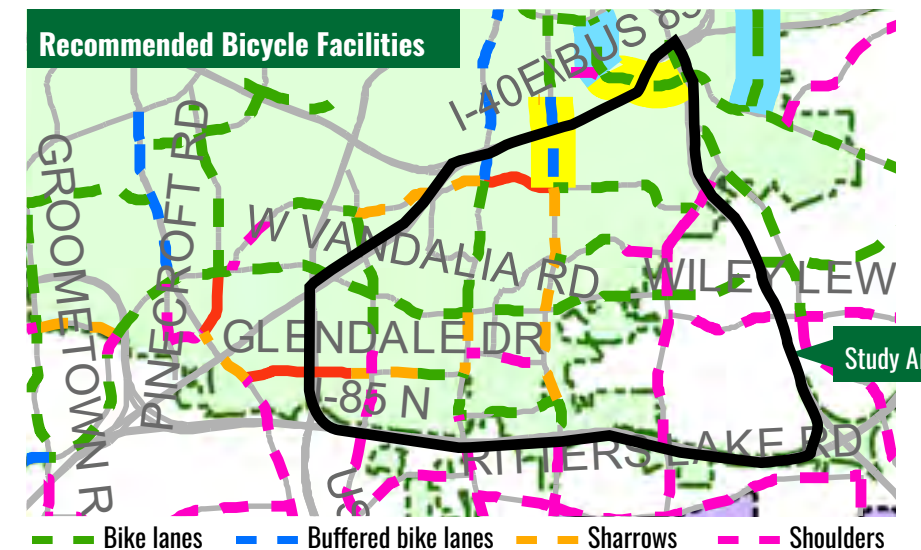
Completed Bicycle Facility Recommendations:

- Sharrows and Shared Lanes on Industrial Ave and Creek Ridge Rd
- Sharrows and Shared Lanes on Glendale Dr (S Holden Rd to Donegal Dr, 0.28 miles)

There are bicycle facility recommendations along the main roads within the Study Area of the South Greensboro Area Plan, including bicycle lanes, sharrows, and shoulders.

Of note:

- On-road Bike facility recommendations generally do not include separated facilities, but standard striped bike lanes or shoulders
- Recommended greenways are Tier 4, which means long term implementation timeline
- Most recommended bicycle facilities are planned for either mid or long term implementation timelines
- Recommended sidewalk projects were planned for 2021 and 2022 timelines, but are now at 2025 timelines



Source of Images: 2018 Greensboro BiPed Plan Amendment

Environment & Services

Environmental

Flood Data

Floods occur naturally and can happen almost anywhere. They may not even be near a body of water, although river and coastal flooding are two of the most common types. Heavy rains in built environments can overwhelm existing drainage networks and put you and your property at risk for flood damage.

Flood maps are one tool that communities use to know which areas have the highest risk of flooding. The Federal Emergency Management Agency (FEMA) maintains and updates flood data and classifies different areas by the likelihood of flooding. But, the nomenclature can be misleading. For example, the 100-year flood area isn't likely to flood once every 100 years. Rather, it may flood fairly regularly after severe rain events, which may occur a dozen times in a year in some areas.




Environmental Compliance Facilities

The EPA monitors regulated facilities through several key systems that collectively form the Environmental Compliance Facilities data. The facilities mapped are monitored by the EPA and regulated under the Clean Air Act and/or the Clean Water Act.

Brownfields

A "brownfields site" is an abandoned, idled or underused property where the threat of environmental contamination has hindered its redevelopment.

 Environmental Compliance Facilities

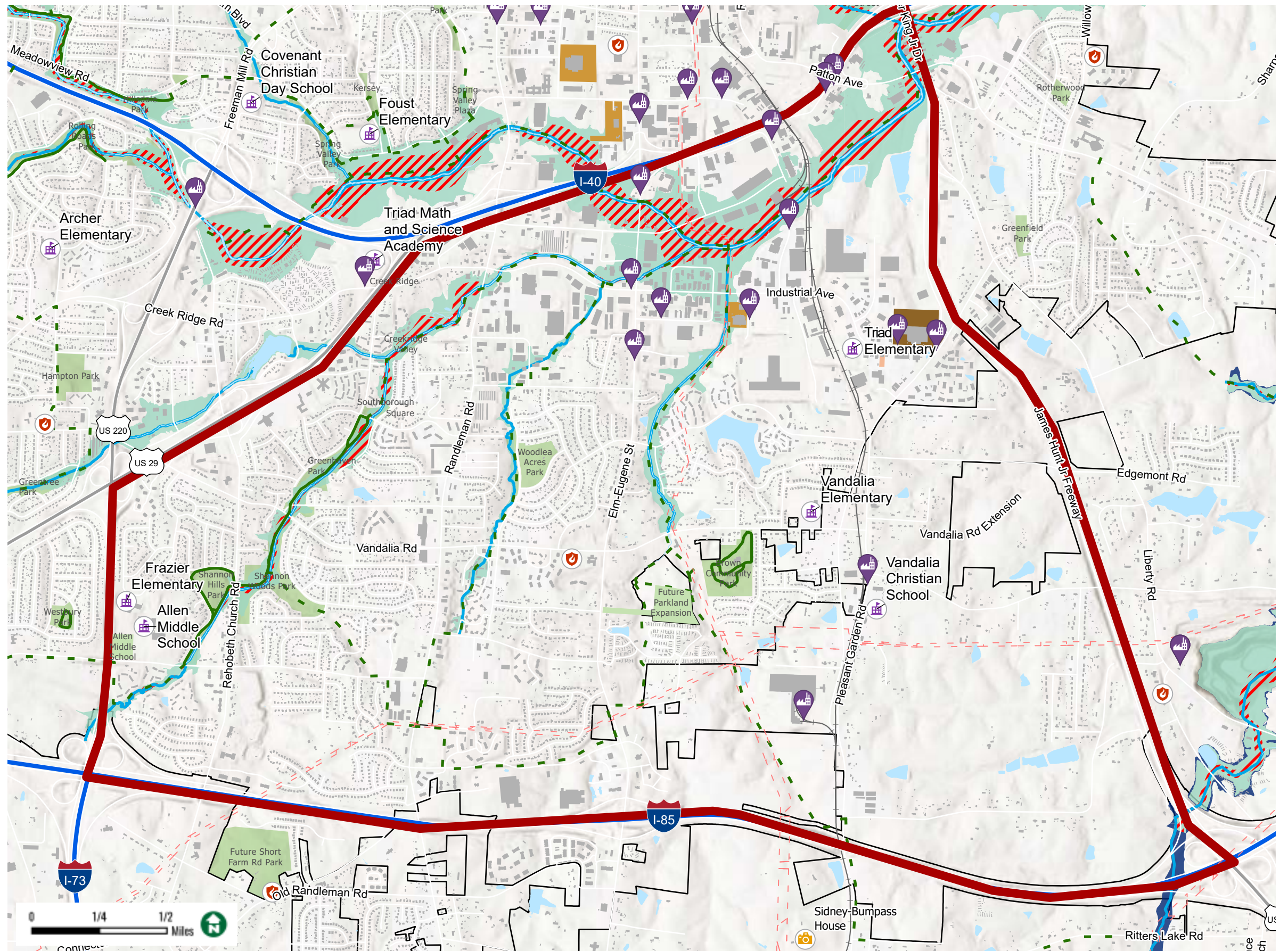
NCDEQ Brownfields
 Active Eligible  Ineligible
 Recorded

 100-year floodplain
 500-year floodplain

 FLOODWAY

 Recommended Greenway




 Trails and Greenways

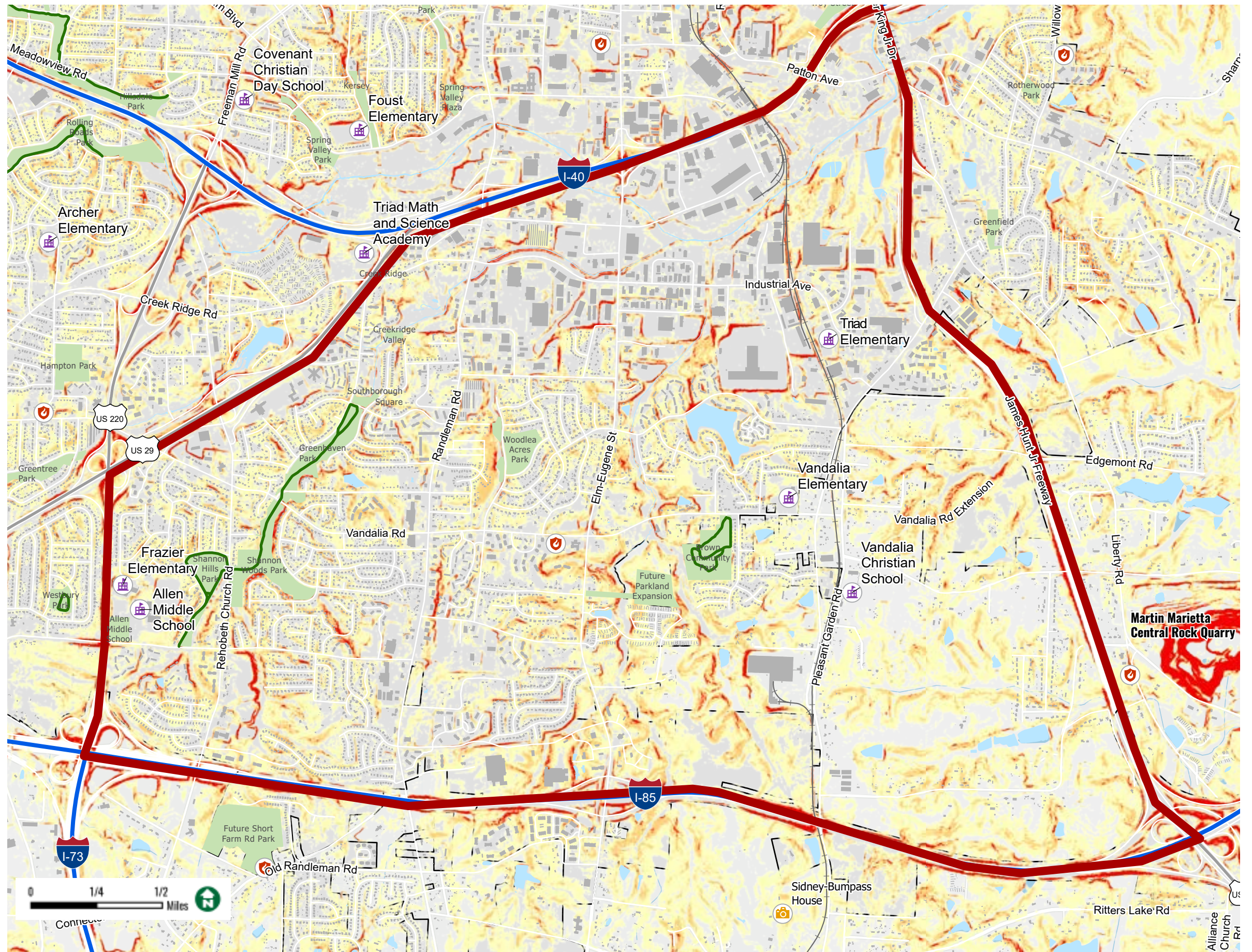


Slope

Sloped terrain can create beautiful vistas, but it can create issues in urban areas for mobility and development. Several portions of the Study Area are sloped to a degree that is difficult for bicyclists and pedestrians and/or not ADA accessible (yellow), and some areas (orange) are sloped even more, which make development difficult and costly. A few locations (red) are sloped to an extent to make development prohibitively costly or not feasible.

- Above 5% slope is more difficult for pedestrians and bicyclists, especially those with mobility challenges
- Slopes above 5-8% not ADA accessible
- Slopes above 12- 15% add significant cost to development
- Above 20% is steep and prohibitive to development

-  **5-12%** - difficult for bicyclists and pedestrians and/or not ADA accessible
-  **12-20%** - not ADA accessible and very challenging for development
-  **20% +** - generally not recommended for development

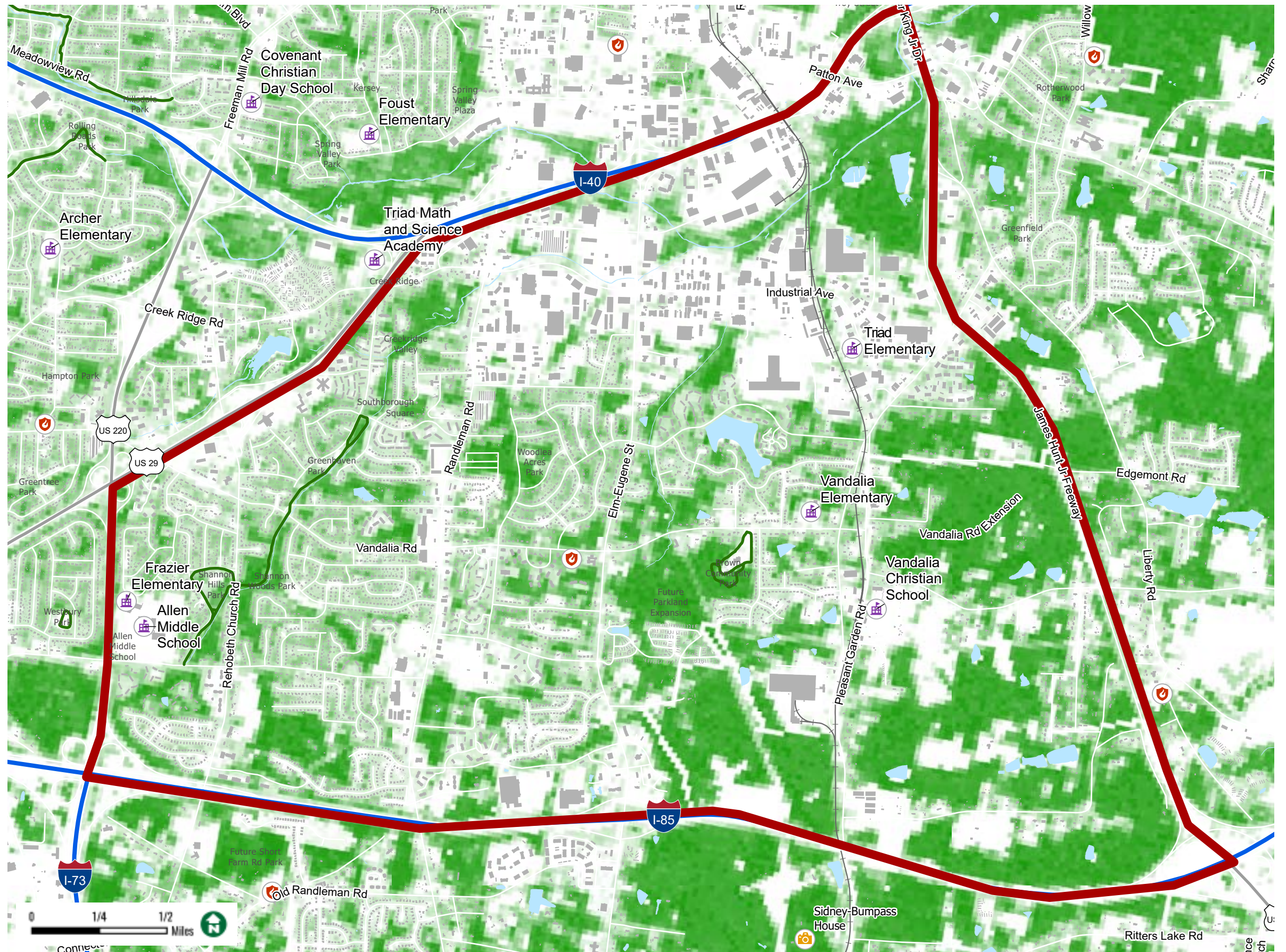


Tree Coverage

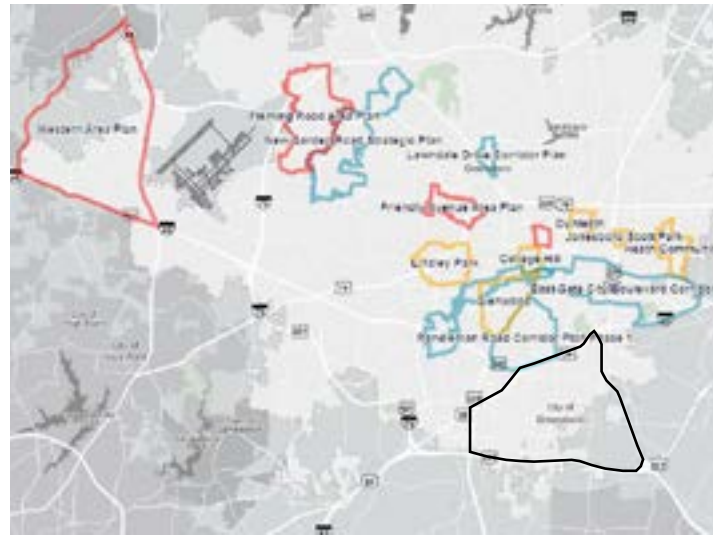
Tree canopy and coverage offer many benefits to communities. A thriving tree canopy is part of the urban infrastructure, providing benefits such as higher quality of life, high property values, healthier communities, lower crime, lower heat, and improved environmental resilience. Areas with generous tree canopy and coverage are often associated with a high quality of life. The Study Area has robust tree coverage generally in the south and southeastern portions, mostly in lightly developed or undeveloped areas.

Tree coverage in existing area neighborhoods is moderate. Given the decades when they were developed, and the fact that areas were likely cleared for development, any trees planted at the time of development would be 40-60 years old by now.

Industrial areas and commercial areas are notably void of any tree coverage.



Citywide Equitable Services Maps



Area Plans

This planning process, which honors and brings forward previous efforts and is anticipated to be adopted after completion, will add South Greensboro to the City's collection of formally adopted planned areas.



Adopted Streams

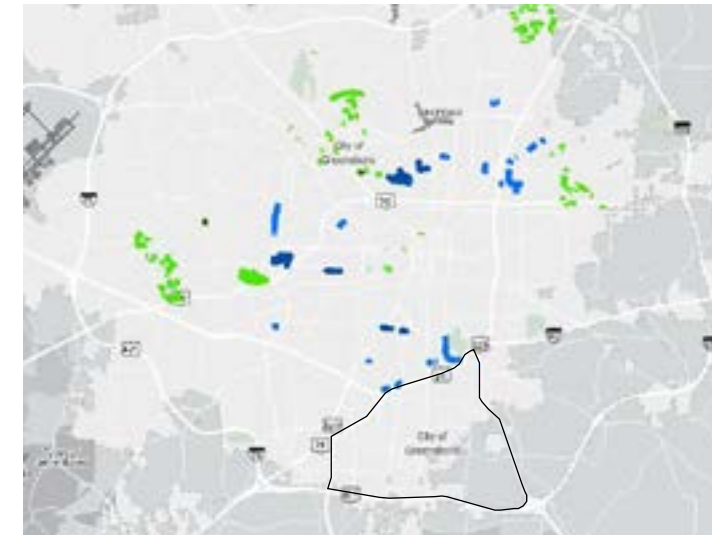
There is only one adopted stream in the Study Area. Compare this to the number of adopted streams in the northern part of Greensboro. Stream adoption is a volunteer effort and open to community businesses and organizations.



Vision Zero (2020)

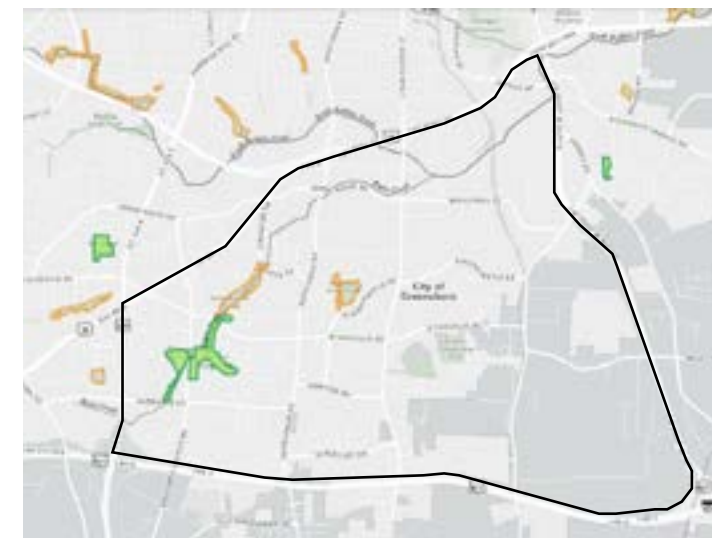
High Injury Network locations are areas identified by data analysis where it is a high priority to invest resources in roadway improvements and other efforts. The intersection of Vandalia Rd and Randleman Rd is a top intersection located in the Study Area.

- Top intersection locations
- HIN priority segments



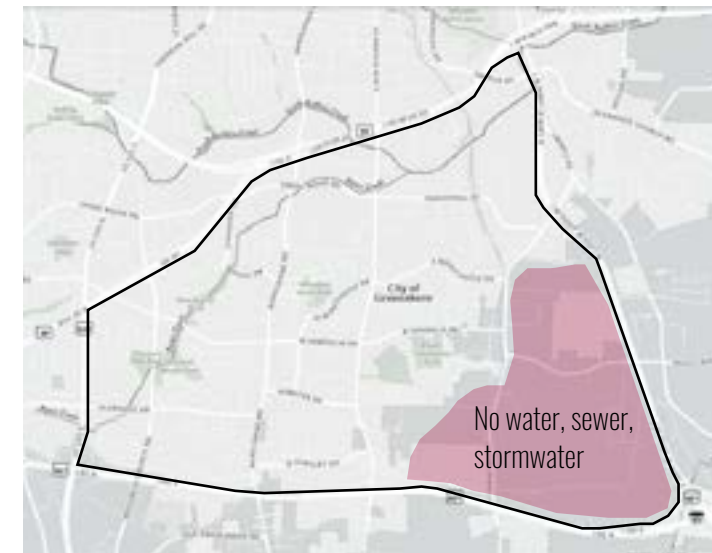
Water and Sewer Rehab Projects

There are currently no active or planned water/sewer rehab projects in the Study Area.



Adopted Parks

There is only one adopted park in the Study Area. Three others are available for adoption. Park adoption is a volunteer effort, open to community businesses and organizations. Are there other open spaces in the Study Area that could be candidates for adoption?



Utilities

Water, sewer and stormwater utilities are generally present throughout the Study Area within the Greensboro city limits. Areas east of Pleasant Garden Road are generally not served with these utilities as they are currently outside city limits.